

**UMATILLA CITY COUNCIL MEETING
AGENDA
COUNCIL CHAMBERS 700 6TH STREET, UMATILLA, OR 97882
AUGUST 2, 2021
7:00 PM**

1. **MEETING CALLED TO ORDER**
2. **ROLL CALL**
3. **PLEDGE OF ALLEGIANCE**
4. **APPROVAL OF AGENDA**
5. **CITY MANAGER'S REPORT**
6. **PUBLIC COMMENT** Public Comment is an opportunity for citizens to express opinions, raise issues, and provide information to the City Council. Comments presented during this segment should be on city-related issues and not on items that are scheduled for a Public Hearing on the same evening's agenda. If you wish to speak, please provide the requested information on the Sign-Up Sheet, being sure to note the topic on which you will speak. When called to the podium, begin by stating your name and address. You will have five minutes to speak, unless otherwise instructed.
7. **CONSENT AGENDA**
 - 7.1 Paid Invoices *Suggested Action: Motion to approve.*
 - 7.2 [Court Report](#) *Suggested Action: Approve May & April*
8. **PUBLIC HEARING**
 - 8.1 [Clever Land Plan Amendment PA-2-20](#): The applicant, Clever Land, LLC, is requesting approval of an Urban Growth Boundary Expansion to include approximately 146.69 acres land. The applicant also submitted an Annexation and Zone Change applications with the desired outcome to have approximately 450 acres of land planned and zoned for industrial use. *Suggested Action: The Umatilla City Council continued Clever Land Plan Amendment PA-2-20 at their July 20, 2021 council meeting and left the record open. The findings and recommendation for Clever Plan Amendment PA-2-20 are attached. A copy of the findings and all exhibits are available on the City's website at the link below or are available at City Hall during normal business hours.*
[Clever Land Plan Amendment PA-2-20](#)

A sample motion for an approval of Clever Land Plan Amendment PA-2-20 is provided below.

I move to approve Clever Land Plan Amendment PA-2-20 and adopt the findings and

recommendation for Cleaver Plan Amendment PA-2-20 as the Councils own.

9. **NEW BUSINESS**

- 9.1 [First Reading of Ordinance No. 848 - An ordinance amending chapter 9 of the City of Umatilla's comprehensive land use plan to incorporate the 2019 Economic Opportunity Analysis and 2018 Destination Management Plan.](#) *Suggested Action: The Council approved City of Umatilla Plan Amendment PA-1-20 at their July 20, 2021, council meeting. Ordinance 848 will implement Plan Amendment PA-1-20 by amending the comprehensive plan. Staff recommends a motion for a first reading of Ordinance No. 848 by title only.*

A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-1-20) is provided below or available at at City Hall during normal business hours.

- 9.2 [Umatilla City Council Report and Decision for Plan Amendment \(PA-1-20\) Adoption of Ordinance No. 848 - An ordinance amending chapter 9 of the City of Umatilla's comprehensive land use plan to incorporate the 2019 Economic Opportunity Analysis and 2018 Destination Management Plan.](#) *Suggested Action: The Council approved City of Umatilla Plan Amendment PA-1-20 at their July 20, 2021, council meeting. Ordinance 848 will implement Plan Amendment PA-1-20 by amending the comprehensive plan. Staff recommends a motion to approve Ordinance No. 848.*

A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-1-20) is provided below or available at at City Hall during normal business hours.

- 9.3 [Umatilla City Council Report and Decision for Plan Amendment \(PA-1-20\) First Reading of Ordinance No. 849 - An ordinance amending the City of Umatilla Comprehensive Plan and Zoning map by changing the plan designation of 294 acres from Single-Family Residential \(R-1\) to Light Industrial \(M-1\)](#) *Suggested Action: The Council approved City of Umatilla Plan Amendment PA-1320 at their July 20, 2021, council meeting. Ordinance 849 will implement Plan Amendment PA-3-20 by amending the Comprehensive Plan and Zoning map. Staff recommend calling for a first reading for Ordinance No. 849 by title only.*

A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-3-20) is provided below or available at at City Hall during normal business hours.

- 9.4 [Umatilla City Council Report and Decision for Plan Amendment \(PA-3-20\) Adoption of Ordinance No. 849 - An ordinance amending the City of Umatilla Comprehensive Plan and Zoning map by changing the plan designation of 294 acres from Single-Family Residential \(R-1\) to Light Industrial \(M-1\)](#) *Suggested Action: The Council approved City of Umatilla Plan Amendment PA-3-20 at their July 20, 2021, council meeting. Ordinance 849 will implement Plan Amendment PA-3-20 by amending the*

Comprehensive Plan and Zoning map.

A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-3-20) is provided below or available at City Hall during normal business hours.

Umatilla City Council Report and Decision for Plan Amendment (PA-3-20)

- 9.5 Resolution No. 06-2022 - A resolution authorizing the city manager to sign a developer agreement for street improvement funds between Fastrack Inc, Columbia Basin Development LLC and the City of Umatilla *Suggested Action: Staff recommends approval of Resolution 06-2021.*

10. **PUBLIC COMMENT**

11. **DISCUSSION ITEMS**

12. **MAYOR'S MESSAGE**

13. **COUNCIL INFORMATION & DISCUSSION**

14. **EXECUTIVE SESSION**

14.2 Potential Real Estate Transaction - ORS 192.660(2)(e) Authorizes council to deliberate with persons designated by council to negotiate real property transactions, including long-term leases. Does not authorize discussion of general leasing policies. *Suggested Action: Discussion Only*

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15. **ADJOURN** This institution is an equal opportunity provider. Discrimination is prohibited by Federal law. Special accommodations to attend or participate in a city meeting or other function can be provided by contacting City Hall at (541) 922-3226 or use the TTY Relay Service at 1-800-735-2900 for appropriate assistance.

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Report Criteria:
Detail report type printed

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------------|----------------|--------------------------|--------------|----------------|--------------|------------------|
| 7 | | | | | | | |
| 7 | A & M Supply | 104318 | Golf Course | 07/03/21 | 77.35 | 47071 | 07/23/21 |
| | | 3047674 | Golf Course | 07/14/21 | 118.76 | 47071 | 07/23/21 |
| Total 7: | | | | | 196.11 | | |
| 8 | | | | | | | |
| 8 | A Burk's Custom Glass | 21181 | Golf Course Pro Shop | 07/14/21 | 610.00 | 47072 | 07/23/21 |
| Total 8: | | | | | 610.00 | | |
| 13 | | | | | | | |
| 13 | A-1 Industrial Hose & Supply | 259327 | Marina B Dock | 07/20/21 | 79.53 | 47073 | 07/23/21 |
| Total 13: | | | | | 79.53 | | |
| 45 | | | | | | | |
| 45 | AJ's Printed Apparel | 19186 | Police Polos | 07/12/21 | 275.00 | 47075 | 07/23/21 |
| Total 45: | | | | | 275.00 | | |
| 102 | | | | | | | |
| 102 | Aramark Uniform Services, Inc. | 5290000129 | Shop Mats and Towels | 04/09/21 | 187.57 | 46992 | 07/08/21 |
| | | 5290000221 | Shop Mats and Towels | 04/23/21 | 187.57 | 46992 | 07/08/21 |
| | | 5290000313 | Police Mats | 05/07/21 | 252.27 | 46992 | 07/08/21 |
| | | 5290000313 | Shop Mats and Towels | 05/07/21 | 187.57 | 46992 | 07/08/21 |
| | | 5290000415 | Police Mats | 05/21/21 | 252.27 | 46992 | 07/08/21 |
| | | 5290000415 | Shop Mats and Towels | 05/21/21 | 205.17 | 46992 | 07/08/21 |
| Total 102: | | | | | 1,272.42 | | |
| 147 | | | | | | | |
| 147 | Banner Bank | SAFEDEPO | Safe Deposit Box | 07/01/21 | 65.00 | 47077 | 07/23/21 |
| Total 147: | | | | | 65.00 | | |
| 148 | | | | | | | |
| 148 | Banner Bank Mastercard | 0715.06.23.2 | Lunch Meetings | 06/23/21 | 13.22 | 46993 | 07/08/21 |
| | | 0715.06.23.2 | Lunch Meetings | 06/23/21 | 13.22 | 46993 | 07/08/21 |
| | | 0715.06.23.2 | Lunch Meetings | 06/23/21 | 13.23 | 46993 | 07/08/21 |
| | | 0715.06.23.2 | Lunch Meetings | 06/23/21 | 13.23 | 46993 | 07/08/21 |
| | | 2217.06.23.2 | TLO Transunion | 06/23/21 | 75.00 | 46993 | 07/08/21 |
| | | 2217.06.23.2 | OPOA-Training-Huxel | 06/23/21 | 475.00 | 46993 | 07/08/21 |
| | | 2217.06.23.2 | NWGIA Refund | 06/23/21 | 75.00 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Photoshop Mo Fee | 06/23/21 | 20.99 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Building Supplies | 06/23/21 | 910.40 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | AED for Marina | 06/23/21 | 1,158.00 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Rec Program Supplies | 06/23/21 | 74.25 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Recreational Advertising | 06/23/21 | 8.97 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Flower Basket Planters | 06/23/21 | 149.96 | 46993 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------|----------------|---|--------------|----------------|--------------|------------------|
| | | 2970.06.23.2 | Buckets for fertiltizer | 06/23/21 | 30.30 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Flags | 06/23/21 | 1,296.00 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Family Fun Day Supplies | 06/23/21 | 41.64 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | NRPLA Membership Registration | 06/23/21 | 1,150.00 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Recreational Equipment | 06/23/21 | 9.22 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | Fertilizer for Hanging Baskets | 06/23/21 | 380.90 | 46993 | 07/08/21 |
| | | 2970.06.23.2 | overlimit fee | 06/23/21 | 20.00 | 46993 | 07/08/21 |
| | | 3132.06.23.2 | Wastewater Lab Probe | 06/23/21 | 180.00 | 46993 | 07/08/21 |
| | | 3132.06.23.2 | Anchors for 6th St Trees | 06/23/21 | 116.52 | 46993 | 07/08/21 |
| | | 3132.06.23.2 | Wastewater CEUs - Tassie | 06/23/21 | 134.75 | 46993 | 07/08/21 |
| | | 3132.06.23.2 | Propane - Employee Appreciation | 06/23/21 | 23.68 | 46993 | 07/08/21 |
| | | 3132.06.23.2 | Pesticide Training Manuals | 06/23/21 | 99.00 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Marina ODA License-Scales | 06/23/21 | 135.00 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Quill Office Supplies | 06/23/21 | 50.13 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Quill Office Supplies | 06/23/21 | 50.13 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Quill Office Supplies | 06/23/21 | 50.13 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Quill Office Supplies | 06/23/21 | 50.13 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Quill Office Supplies | 06/23/21 | 50.14 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Quill Office Supplies | 06/23/21 | 50.13 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Quill Office Supplies | 06/23/21 | 50.13 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Fastenal | 06/23/21 | .78 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Library Programming | 06/23/21 | 129.94 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Garbage Bags for Parks | 06/23/21 | 121.14 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Garbage Bags for parks | 06/23/21 | 30.49 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Utility Clerk Meeting | 06/23/21 | 20.80 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Utility Clerk Meeting | 06/23/21 | 20.80 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Front Office Lunch Meeting | 06/23/21 | 20.80 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Golf Course supplies | 06/23/21 | 55.98 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Lab Suppliles-WWTP | 06/23/21 | 95.80 | 46993 | 07/08/21 |
| | | 3960.06.23.2 | Golf Course Supplies | 06/23/21 | 21.90 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | Rec. Luncheon | 06/23/21 | 311.08 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | New Building code books | 06/23/21 | 219.00 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | City Council/Planning Land Use Training | 06/23/21 | 13.98 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | City Council/Planning Land Use Training | 06/23/21 | 50.72 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | City Council/Planning Land Use Training | 06/23/21 | 270.50 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | City Council/Planning Land Use Training | 06/23/21 | 35.90 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | Rec. Programming supplies | 06/23/21 | 71.11 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | Property Deeds from County | 06/23/21 | 1.50 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | Property Deeds from County | 06/23/21 | 4.25 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | Walmart Credit Voucher | 06/23/21 | 16.15- | 46993 | 07/08/21 |
| | | 5571.06.23.2 | OBOA Membership | 06/23/21 | 225.00 | 46993 | 07/08/21 |
| | | 5571.06.23.2 | Work Lunch PW + Parks | 06/23/21 | 45.25 | 46993 | 07/08/21 |
| | | 5919.06.23.2 | Indeed Job Postings | 06/23/21 | 1,259.00 | 46993 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------------------------------|----------------|-------------------------------------|--------------|----------------|--------------|------------------|
| | | 5919.06.23.2 | Marina Office Supplies | 06/23/21 | 40.96 | 46993 | 07/08/21 |
| | | 5919.06.23.2 | Callaway Golf Order | 06/23/21 | 453.49 | 46993 | 07/08/21 |
| | | 5919.06.23.2 | Moorage Keys | 06/23/21 | 39.80 | 46993 | 07/08/21 |
| | | 5919.06.23.2 | RV Office Supplies | 06/23/21 | 436.32 | 46993 | 07/08/21 |
| | | 5919.06.23.2 | CPE Review | 06/23/21 | 399.00 | 46993 | 07/08/21 |
| | | 5919.06.23.2 | Frito Lay Golf Order | 06/23/21 | 293.47 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | Marina Phone Case | 06/23/21 | 44.16 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | Employee Appreciation Event | 06/23/21 | 456.48 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | OAMR 2021 Membership | 06/23/21 | 60.00 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 37.81 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 12.60 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 12.60 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 12.60 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 390.75 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 12.60 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 37.82 | 46993 | 07/08/21 |
| | | 8328.06.23.2 | City Phone Bill | 06/23/21 | 12.62 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Amazon-Trash bags | 06/23/21 | 60.25 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Amazon-Duty Belt Liner | 06/23/21 | 14.50 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Amazon-HP Ink | 06/23/21 | 50.58 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Pizza Hut-Staff Mtg.-training | 06/23/21 | 70.96 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Amazon-Smead Card Guide Tabs | 06/23/21 | 22.35 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | amazon-File Folders & Facial Tissue | 06/23/21 | 117.96 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Tactical Pants Code Enforcement | 06/23/21 | 81.98 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Amazon-Compressed Gas | 06/23/21 | 6.29 | 46993 | 07/08/21 |
| | | 8336.06.23.2 | Pizza Hut Savings | 06/23/21 | 2.84 | 46993 | 07/08/21 |
| | | | | | <hr/> | | |
| | | | | | 13,003.08 | | |
| | | | | | <hr/> | | |
| 231 | | | | | | | |
| 231 | Bonney's AG & Auto Repair | 06242021 | Golf Course | 06/24/21 | 73.00 | 47080 | 07/23/21 |
| | | | | | <hr/> | | |
| | | | | | 73.00 | | |
| | | | | | <hr/> | | |
| 276 | | | | | | | |
| 276 | Builders FirstSource | 83625960 | Golf Course | 06/25/21 | 264.81 | 47082 | 07/23/21 |
| | | 83646713 | Golf Course | 06/25/21 | 135.97 | 47082 | 07/23/21 |
| | | | | | <hr/> | | |
| | | | | | 400.78 | | |
| | | | | | <hr/> | | |
| 320 | | | | | | | |
| 320 | Canon Solutions America, Inc | 4036415181 | COPIER | 05/29/21 | 204.69 | 46995 | 07/08/21 |
| | | 4036645990 | COPIER MAINTENANCE | 06/24/21 | 58.80 | 47083 | 07/23/21 |
| | | 4036695191 | COPIER MAINTENANCE | 06/30/21 | 582.58 | 47083 | 07/23/21 |
| | | | | | <hr/> | | |
| | | | | | 846.07 | | |
| | | | | | <hr/> | | |
| 351 | | | | | | | |
| 351 | Cascade Natural Gas Corp. | 1092.06.23.2 | 700 6th St. | 06/23/21 | 4.66 | 46997 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------------|----------------|---|--------------|----------------|--------------|------------------|
| | | 1092.06.23.2 | 700 6th St. | 06/23/21 | 4.66 | 46997 | 07/08/21 |
| | | 1092.06.23.2 | 700 6th St. | 06/23/21 | 4.66 | 46997 | 07/08/21 |
| | | 3033.06.23.2 | 82959 Draper St. | 06/23/21 | 12.61 | 46997 | 07/08/21 |
| | | 7846.06.24.2 | 830 6th St. | 06/24/21 | 33.74 | 46997 | 07/08/21 |
| | | 7851.06.24.2 | 822 6TH ST. | 06/24/21 | 12.61 | 46997 | 07/08/21 |
| | | 8476.06.24.2 | 1205 W. 3RD St. | 06/24/21 | 32.37 | 46997 | 07/08/21 |
| | Total 351: | | | | 105.31 | | |
| 353 | | | | | | | |
| 353 | Caselle, Inc. | 110235 | Annual Software Support | 06/01/21 | 17,157.00 | 46983 | 07/01/21 |
| | Total 353: | | | | 17,157.00 | | |
| 355 | | | | | | | |
| 355 | Casiday Battery Co. | 12988 | Golf Course | 04/29/21 | 129.95 | 46998 | 07/08/21 |
| | Total 355: | | | | 129.95 | | |
| 361 | | | | | | | |
| 361 | CDW Government | ORDERMFX | 3D Printer MakerBot Education Starter Kit | 07/06/21 | 1,896.95 | 46999 | 07/08/21 |
| | Total 361: | | | | 1,896.95 | | |
| 362 | | | | | | | |
| 362 | Center Point Large Print | 1859941 | Large Print Books for Library | 07/01/21 | 140.22 | 47085 | 07/23/21 |
| | Total 362: | | | | 140.22 | | |
| 367 | | | | | | | |
| 367 | CenturyLink | 678B06.25.2 | Police Dept Phones | 06/25/21 | 90.24 | 47000 | 07/08/21 |
| | Total 367: | | | | 90.24 | | |
| 391 | | | | | | | |
| 391 | CI INFORMATION MANAGMEN | 0115358 | Onsite document shred | 03/31/21 | 100.65 | 47086 | 07/23/21 |
| | | 0119924 | PD Shred Services | 06/30/21 | 49.29 | 47086 | 07/23/21 |
| | | 0119925 | Onsite document shred | 06/30/21 | 98.59 | 47086 | 07/23/21 |
| | Total 391: | | | | 248.53 | | |
| 394 | | | | | | | |
| 394 | City County Insurance Services | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 4,033.20 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 2,319.09 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 1,209.96 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 12,099.60 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 7,562.25 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 37,307.10 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 49,406.70 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 13,107.91 | 47001 | 07/08/21 |
| | | PO-UMA-I20 | Comp & Liab Insurance | 07/01/21 | 12,099.60 | 47001 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------------------------|----------------|------------------------------------|--------------|----------------|--------------|------------------|
| Total 394: | | | | | 139,145.41 | | |
| 427 | | | | | | | |
| 427 | Columbia Harvest Foods | 01-1343839 | Public Works Drinks | 01/20/21 | 15.53 | 47004 | 07/08/21 |
| | | 01-1367704 | Public Works | 03/03/21 | 9.69 | 47004 | 07/08/21 |
| | | 01-1367704 | Public Works | 03/03/21 | 9.69 | 47004 | 07/08/21 |
| | | 01-1367704 | Public Works | 03/03/21 | 9.68 | 47004 | 07/08/21 |
| | | 01-1367704 | Public Works | 03/03/21 | 9.68 | 47004 | 07/08/21 |
| | | 01-1368742 | Valve Boxes | 03/05/21 | 12.08 | 47004 | 07/08/21 |
| | | 01-1375416 | Public Works Drinks | 03/16/21 | 10.01 | 47004 | 07/08/21 |
| | | 01-1375416 | Public Works Drinks | 03/16/21 | 10.01 | 47004 | 07/08/21 |
| | | 01-1375416 | Public Works Drinks | 03/16/21 | 10.01 | 47004 | 07/08/21 |
| | | 01-1375416 | Public Works Drinks | 03/16/21 | 10.00 | 47004 | 07/08/21 |
| | | 01-1375416 | Public Works Drinks | 03/16/21 | 10.00 | 47004 | 07/08/21 |
| | | 01-1375416 | Public Works Drinks | 03/16/21 | 10.00 | 47004 | 07/08/21 |
| | | 01-1375416 | lab supplies | 03/16/21 | 16.99 | 47004 | 07/08/21 |
| | | 01-1395398 | Dept. Valve Maint. | 04/21/21 | 9.29 | 47004 | 07/08/21 |
| | | 01-1407340 | lab supplies | 05/11/21 | 7.14 | 47004 | 07/08/21 |
| | | 01-1407340 | Public Works | 05/11/21 | 2.79 | 47004 | 07/08/21 |
| | | 01-1407340 | Public Works | 05/11/21 | 2.79 | 47004 | 07/08/21 |
| | | 01-1407340 | Public Works | 05/11/21 | 2.80 | 47004 | 07/08/21 |
| | | 01-1407340 | Public Works | 05/11/21 | 2.80 | 47004 | 07/08/21 |
| | | 01-1407340 | Public Works | 05/11/21 | 2.80 | 47004 | 07/08/21 |
| | | 01-1407340 | Public Works | 05/11/21 | 2.80 | 47004 | 07/08/21 |
| | | 01-1407340 | Public Works | 05/11/21 | 2.80 | 47004 | 07/08/21 |
| | | 02.1797237 | Golf Well | 02/03/21 | 29.65 | 47004 | 07/08/21 |
| | | 02-1752913 | Public Works Drinks | 12/01/20 | 15.53 | 47004 | 07/08/21 |
| | | 02-1767161 | Public Works Drinks | 12/22/20 | 8.50 | 47004 | 07/08/21 |
| | | 02-1767161 | Public Works Drinks | 12/22/20 | 8.50 | 47004 | 07/08/21 |
| | | 02-1767161 | Public Works Drinks | 12/22/20 | 8.50 | 47004 | 07/08/21 |
| | | 02-1767161 | Public Works Drinks | 12/22/20 | 8.50 | 47004 | 07/08/21 |
| | | 02-1767161 | lab supplies | 12/22/20 | 35.73 | 47004 | 07/08/21 |
| | | 02-1806026 | Public Works Drinks | 02/16/21 | 13.27 | 47004 | 07/08/21 |
| | | 02-1806026 | Public Works Drinks | 02/16/21 | 13.26 | 47004 | 07/08/21 |
| | | 02-1806026 | Public Works Drinks | 02/16/21 | 13.26 | 47004 | 07/08/21 |
| | | 02-1806026 | Public Works Drinks | 02/16/21 | 13.26 | 47004 | 07/08/21 |
| | | 02-1824127 | Tree Planting Supplies | 03/13/21 | 11.96 | 47004 | 07/08/21 |
| | | 02-1859042 | Public Works | 05/01/21 | 2.80 | 47004 | 07/08/21 |
| | | 02-1859042 | Public Works | 05/01/21 | 2.80 | 47004 | 07/08/21 |
| | | 02-1859042 | Public Works | 05/01/21 | 2.80 | 47004 | 07/08/21 |
| | | 02-1859042 | Public Works | 05/01/21 | 2.79 | 47004 | 07/08/21 |
| | | 02-1859042 | Public Works | 05/01/21 | 2.79 | 47004 | 07/08/21 |
| | | 02-1859042 | Public Works | 05/01/21 | 2.79 | 47004 | 07/08/21 |
| | | 02-1880788 | Batteries for door locks at Marina | 05/30/21 | 8.35 | 47004 | 07/08/21 |
| | | 03-1289449 | Public Works Drinks | 03/31/21 | 5.80 | 47004 | 07/08/21 |
| | | 03-1289449 | Public Works Drinks | 03/31/21 | 5.81 | 47004 | 07/08/21 |
| | | 03-1289449 | Public Works Drinks | 03/31/21 | 5.80 | 47004 | 07/08/21 |
| | | 03-1289449 | Public Works Drinks | 03/31/21 | 5.80 | 47004 | 07/08/21 |
| | | 03-1289449 | Public Works Drinks | 03/31/21 | 5.80 | 47004 | 07/08/21 |
| | | 03-1289449 | Public Works Drinks | 03/31/21 | 5.80 | 47004 | 07/08/21 |
| | | 03-1289449 | lab supplies | 03/31/21 | 37.39 | 47004 | 07/08/21 |
| | | 03-1301736 | Public Works | 04/21/21 | 8.69 | 47004 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------------------------|----------------|------------------------------|--------------|----------------|--------------|------------------|
| | | 03-1301736 | Public Works | 04/21/21 | 8.70 | 47004 | 07/08/21 |
| | | 03-1301736 | Public Works | 04/21/21 | 8.69 | 47004 | 07/08/21 |
| | | 03-1301736 | Public Works | 04/21/21 | 8.69 | 47004 | 07/08/21 |
| | | 03-1301736 | Public Works | 04/21/21 | 8.69 | 47004 | 07/08/21 |
| | | 03-1301736 | Public Works | 04/21/21 | 8.69 | 47004 | 07/08/21 |
| | | 03-1301736 | lab supplies | 04/21/21 | 18.54 | 47004 | 07/08/21 |
| | | 03-1308806 | Rec. Program | 05/01/21 | 93.07 | 47004 | 07/08/21 |
| | | 04-1266166 | First Aid Training | 01/15/21 | 2.83 | 47004 | 07/08/21 |
| | | 04-1266166 | First Aid Training | 01/15/21 | 2.83 | 47004 | 07/08/21 |
| | | 04-1266166 | First Aid Training | 01/15/21 | 2.83 | 47004 | 07/08/21 |
| | | 04-1266166 | First Aid Training | 01/15/21 | 2.84 | 47004 | 07/08/21 |
| | | 04-1266166 | First Aid Training | 01/15/21 | 2.83 | 47004 | 07/08/21 |
| | | 04-1266166 | First Aid Training | 01/15/21 | 2.83 | 47004 | 07/08/21 |
| | | 04-1277595 | Public Works Drinks | 02/05/21 | 2.32 | 47004 | 07/08/21 |
| | | 04-1277595 | Public Works Drinks | 02/05/21 | 2.33 | 47004 | 07/08/21 |
| | | 04-1277595 | Public Works Drinks | 02/05/21 | 2.32 | 47004 | 07/08/21 |
| | | 04-1277595 | Public Works Drinks | 02/05/21 | 2.32 | 47004 | 07/08/21 |
| | | 04-1293774 | Water for Tree Planting | 03/10/21 | 55.90 | 47004 | 07/08/21 |
| Total 427: | | | | | 699.76 | | |
| 487 | | | | | | | |
| 487 | Crystal Clear Ice, LLC | 20-102922 | Ice for Marina Resale | 07/05/21 | 134.40 | 47090 | 07/23/21 |
| | | 20-102959 | Ice for Marina Resale | 07/08/21 | 81.20 | 47090 | 07/23/21 |
| | | 20-103017 | Ice for Marina Resale | 07/12/21 | 75.60 | 47090 | 07/23/21 |
| | | 20-103115 | Ice for Marina Resale | 07/19/21 | 63.00 | 47090 | 07/23/21 |
| | | 23-104403 | Ice for Marina Resale | 06/14/21 | 98.00 | 47006 | 07/08/21 |
| | | 23-104516 | Ice for Marina Resale | 06/24/21 | 126.00 | 47090 | 07/23/21 |
| | | INV21-10650 | Ice for Marina Resale | 06/29/21 | 117.60 | 47006 | 07/08/21 |
| Total 487: | | | | | 695.80 | | |
| 488 | | | | | | | |
| 488 | Crystal Springs | 2150670707 | Water for Marina | 07/01/21 | 36.92 | 47007 | 07/08/21 |
| | | 9262940 062 | Water for Police Department | 06/23/21 | 48.91 | 47007 | 07/08/21 |
| Total 488: | | | | | 85.83 | | |
| 525 | | | | | | | |
| 525 | DCBS - Fiscal Services | QTR.2-2021 | Bldg Surcharge Quarterly Sub | 07/06/21 | 27,808.22 | 47008 | 07/08/21 |
| Total 525: | | | | | 27,808.22 | | |
| 540 | | | | | | | |
| 540 | DEMCO, Inc. | 6975800 | Bar Code Supplies Library | 07/12/21 | 117.50 | 47091 | 07/23/21 |
| Total 540: | | | | | 117.50 | | |
| 577 | | | | | | | |
| 577 | DIVCO | SCPAY87820 | Commerical HVAC-City Hall | 07/01/21 | 484.50 | 47093 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|-----------------------------|----------------|---|--------------|----------------|--------------|------------------|
| | | SCPAY87821 | Public Works HVAC | 07/01/21 | 146.00 | 47093 | 07/23/21 |
| | | SCPAY87822 | WWTP HVAC | 07/01/21 | 353.75 | 47093 | 07/23/21 |
| | | SVC83046 | Big River Golf Course | 06/23/21 | 136.90 | 47009 | 07/08/21 |
| Total 577: | | | | | 1,121.15 | | |
| 609 | | | | | | | |
| 609 | Duke's Auto Plus | 13727 | A/C Maintenance-Dodge Charger | 06/24/21 | 120.00 | 47010 | 07/08/21 |
| | | 13741 | Dodge Charger-AC | 06/30/21 | 155.00 | 47010 | 07/08/21 |
| Total 609: | | | | | 275.00 | | |
| 628 | | | | | | | |
| 628 | East Oregonian | 246706 | Archery Instructor | 05/27/21 | 546.10 | 47095 | 07/23/21 |
| | | 248937 | supplemental budget meeting notice | 06/10/21 | 131.50 | 47011 | 07/08/21 |
| | | 250208 | PUBLIC NOTICE | 06/29/21 | 660.06 | 47095 | 07/23/21 |
| | | 252971 | PUBLIC NOTICE | 07/10/21 | 349.74 | 47095 | 07/23/21 |
| | | FC06302021 | Advertising | 07/01/21 | 37.26 | 47095 | 07/23/21 |
| Total 628: | | | | | 1,724.66 | | |
| 635 | | | | | | | |
| 635 | Eastern Oregon Telecom, LLC | 0317.7.01.20 | Golf Course | 07/01/21 | 103.34 | 47012 | 07/08/21 |
| | | 8743.07.01.0 | City Hall Internet | 07/01/21 | 258.90 | 47012 | 07/08/21 |
| | | 8743.07.01.0 | Marina Internet | 07/01/21 | 229.33 | 47012 | 07/08/21 |
| | | 8743.07.01.0 | Shop Internet | 07/01/21 | 265.04 | 47012 | 07/08/21 |
| | | 8743.07.01.0 | City Shop | 07/01/21 | 476.70 | 47012 | 07/08/21 |
| | | 8743.07.01.0 | City Hall Internet | 07/01/21 | 117.48 | 47012 | 07/08/21 |
| | | 8743.07.01.0 | Police Dept. Internet | 07/01/21 | 172.86 | 47012 | 07/08/21 |
| | | 8743.07.01.0 | City Hall Internet | 07/01/21 | 86.30 | 47012 | 07/08/21 |
| Total 635: | | | | | 1,709.95 | | |
| 646 | | | | | | | |
| 646 | Edson International | 6635541 | Marina Suction Nozzle with Splash Guard | 06/25/21 | 194.50 | 47013 | 07/08/21 |
| Total 646: | | | | | 194.50 | | |
| 659 | | | | | | | |
| 659 | Elmer's Irrigation & Supply | 303539 | Golf Course Supplies | 07/13/21 | 96.79 | 47096 | 07/23/21 |
| | | 303821 | Irrigation Supplies | 07/20/21 | 85.96 | 47096 | 07/23/21 |
| Total 659: | | | | | 182.75 | | |
| 755 | | | | | | | |
| 755 | Foreman, Christopher | OAWU2021 | OAWU Conference | 08/22/21 | 319.50 | 47097 | 07/23/21 |
| Total 755: | | | | | 319.50 | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------------------------------|----------------|---------------------------------|--------------|----------------|--------------|------------------|
| 854 | | | | | | | |
| 854 | Gordon's Electric Inc. | J000616 | Sixth St. L St to Switzler Ave. | 07/06/21 | 11,515.00 | 47101 | 07/23/21 |
| | | W15663 | WWTP-Aerator Blower | 06/24/21 | 110.00 | 47015 | 07/08/21 |
| | | W15700 | Marina RV Park | 07/06/21 | 283.91 | 47101 | 07/23/21 |
| | | W15746 | Marina RV Park | 07/15/21 | 295.00 | 47101 | 07/23/21 |
| Total 854: | | | | | 12,203.91 | | |
| 856 | | | | | | | |
| 856 | Gotcha Covered | 137749 | Cleaning Services | 07/07/21 | 443.84 | 47016 | 07/08/21 |
| | | 137749 | Cleaning Services | 07/07/21 | 383.98 | 47016 | 07/08/21 |
| | | 137749 | Cleaning Services | 07/07/21 | 383.98 | 47016 | 07/08/21 |
| | | 137749 | Cleaning Services | 07/07/21 | 248.20 | 47016 | 07/08/21 |
| Total 856: | | | | | 1,460.00 | | |
| 864 | | | | | | | |
| 864 | Granite Construction Company | 2030850 | Marina Gravel | 06/08/21 | 119.05 | 47017 | 07/08/21 |
| | | 2031518 | golf Course Paths | 06/09/21 | 253.26 | 47017 | 07/08/21 |
| | | 2031518 | Marina Gravel | 06/09/21 | 126.08 | 47017 | 07/08/21 |
| | | 2035739 | Marina Gravel | 06/16/21 | 841.95 | 47102 | 07/23/21 |
| | | 2036980 | golf Course Paths | 06/17/21 | 502.38 | 47102 | 07/23/21 |
| | | 2045905 | Marina Gravel | 06/29/21 | 232.46 | 47102 | 07/23/21 |
| | | 2048674 | Marina Gravel | 06/30/21 | 603.75 | 47102 | 07/23/21 |
| Total 864: | | | | | 2,678.93 | | |
| 905 | | | | | | | |
| 905 | H.D. Fowler Company | I5814817 | Water Dept Supplies Meters etc | 06/16/21 | 663.44 | 47104 | 07/23/21 |
| | | I5821972 | Water Dept Supplies Meters etc | 06/23/21 | 366.62 | 47018 | 07/08/21 |
| | | I5829273 | Water Dept Supplies Meters etc | 07/01/21 | 597.15 | 47104 | 07/23/21 |
| | | I5836592 | Meter box HDPE Cover | 07/09/21 | 301.52 | 47104 | 07/23/21 |
| Total 905: | | | | | 1,928.73 | | |
| 960 | | | | | | | |
| 960 | Carson | 110929 | Golf Course | 07/01/21 | 599.16 | 46996 | 07/08/21 |
| | | 110930 | Gas for Public Works Vehicles | 07/01/21 | 429.00 | 46996 | 07/08/21 |
| | | 110930 | Gas for Public Works Vehicles | 07/01/21 | 700.27 | 46996 | 07/08/21 |
| | | 110930 | Gas for Public Works Vehicles | 07/01/21 | 277.59 | 46996 | 07/08/21 |
| | | 110930 | Gas for Public Works Vehicles | 07/01/21 | 696.06 | 46996 | 07/08/21 |
| | | 113016 | Big River Golf Fuel | 06/30/21 | 881.01 | 46996 | 07/08/21 |
| | | 37420 | Police Dept Fuel | 07/01/21 | 2,287.22 | 46996 | 07/08/21 |
| | | 37421 | Gas for Public Works Vehicles | 06/21/21 | 290.83 | 47084 | 07/23/21 |
| | | 37421 | Gas for Public Works | | | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
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| | | 493268 | Oil Change | 07/12/21 | 48.95 | 47108 | 07/23/21 |
| | | 493506 | Oil Change | 07/14/21 | 47.95 | 47108 | 07/23/21 |
| Total 980: | | | | | 181.80 | | |
| 994 | | | | | | | |
| 994 | High Performance Signs | 25246 | Golf Course Signage | 06/18/21 | 1,020.00 | 47022 | 07/08/21 |
| Total 994: | | | | | 1,020.00 | | |
| 1012 | | | | | | | |
| 1012 | Home Depot Credit Services | 3133827 | Marina Supplies | 05/29/21 | 74.40 | 47024 | 07/08/21 |
| | | 3134395 | Marina Supplies | 07/08/21 | 95.36 | 47024 | 07/08/21 |
| | | 4040176 | Parks Supplies | 06/17/21 | 22.46 | 47024 | 07/08/21 |
| | | 5254248 | Marina Supplies | 06/16/21 | 190.25 | 47024 | 07/08/21 |
| | | 6903984 | Marina Supplies | 06/15/21 | 674.00 | 47024 | 07/08/21 |
| | | 7103955 | 6th Street Flags | 06/14/21 | 189.85 | 47024 | 07/08/21 |
| | | 7521987 | Marina Tools | 06/04/21 | 592.43 | 47024 | 07/08/21 |
| Total 1012: | | | | | 1,838.75 | | |
| 1034 | | | | | | | |
| 1034 | HRA VEBA Trust Contributions | 07.2021 | HRA Contribution-Campos | 07/21/21 | 600.00 | 47111 | 07/23/21 |
| | | 072021HOR | HRA VEBA Contribution-Horman | 07/21/21 | 400.00 | 47111 | 07/23/21 |
| Total 1034: | | | | | 1,000.00 | | |
| 1060 | | | | | | | |
| 1060 | Ingram | 53684443 | Library Books | 07/06/21 | 17.39 | 47113 | 07/23/21 |
| | | 53684444 | Library Books | 07/06/21 | 16.19 | 47113 | 07/23/21 |
| | | 53684445 | Library Books | 07/06/21 | 16.20 | 47113 | 07/23/21 |
| | | 53684446 | Library Books | 07/06/21 | 14.97 | 47113 | 07/23/21 |
| | | 53684447 | Library Books | 07/06/21 | 9.75 | 47113 | 07/23/21 |
| | | 53684448 | Library Books | 07/06/21 | 10.36 | 47113 | 07/23/21 |
| | | 53684449 | Library Books | 07/06/21 | 138.74 | 47113 | 07/23/21 |
| | | 53684450 | Library Books | 07/06/21 | 73.28 | 47113 | 07/23/21 |
| Total 1060: | | | | | 296.88 | | |
| 1068 | | | | | | | |
| 1068 | Intermountain ESD | 525P020879 | RV park brochures | 06/24/21 | 577.83 | 47025 | 07/08/21 |
| | | 598T072501 | WWTP Computer Equipment | 07/02/21 | 2,227.44 | 47025 | 07/08/21 |
| | | 598T072562 | Surface case | 07/09/21 | 47.62 | 47114 | 07/23/21 |
| | | 598T072640 | Plantronics-wireless headsets | 07/09/21 | 393.58 | 47114 | 07/23/21 |
| | | 598T072798 | Standard UCC SSL up to 5 domains | 06/14/21 | 439.98 | 47114 | 07/23/21 |
| Total 1068: | | | | | 3,686.45 | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
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| 1089 | | | | | | | |
| 1089 | J U B Engineers, Inc. | 144091 | Wastewater Facilities Plan | 06/24/21 | 13,837.44 | 46985 | 07/01/21 |
| | | 144093 | Wanapa Road and Utilities Extension | 06/24/21 | 19,820.72 | 46985 | 07/01/21 |
| | | 144094 | Power City-Brownell Water Improvements | 06/24/21 | 8,270.18 | 46985 | 07/01/21 |
| | | 144098 | Pedestrian Bridge and Waterline Replacement | 06/24/21 | 235.85 | 46985 | 07/01/21 |
| | | 144098 | Pedestrian Bridge and Waterline Replacement | 06/24/21 | 2,162.85 | 46985 | 07/01/21 |
| | | 144436 | Umatilla Water Master Plan | 07/12/21 | 11,871.43 | 47115 | 07/23/21 |
| | | 144499 | Umatilla Public Works Standards Update | 07/14/21 | 1,148.63 | 47115 | 07/23/21 |
| | | 144499 | Umatilla Public Works Standards Update | 07/14/21 | 1,148.63 | 47115 | 07/23/21 |
| | | 144499 | Umatilla Public Works Standars Update | 07/14/21 | 1,148.64 | 47115 | 07/23/21 |
| | | 144533 | Umatilla On-Call Engineering Services | 07/14/21 | 1,356.01 | 47115 | 07/23/21 |
| | | 144533 | Umatilla On-Call Engineering Services | 07/14/21 | 1,356.01 | 47115 | 07/23/21 |
| | | 144537 | Umatilla Phase 1 Construction Management Services | 07/14/21 | 4,159.32 | 47115 | 07/23/21 |
| | | 144541 | Umatilla Development Engineering Reviews | 07/14/21 | 298.51 | 47115 | 07/23/21 |
| | | 144546 | Umatilla Wastewater Facilities Plan | 07/14/21 | 5,170.04 | 47115 | 07/23/21 |
| | | 144550 | Umatilla-6th Street Improvements L to Switzler | 07/14/21 | 1,481.90 | 47115 | 07/23/21 |
| | | 144552 | Wanapa Road and Utilities Extension | 07/14/21 | 32,321.94 | 47115 | 07/23/21 |
| | | 144556 | Power City-Brownell Water Improvements | 07/14/21 | 22,132.40 | 47115 | 07/23/21 |
| | | 144559 | CityofUmatilla-Risk and Resilience Assessment | 07/14/21 | 4,736.50 | 47115 | 07/23/21 |
| | | 144571 | Umatilla Pedestrian Bridge and Waterline Replacement | 07/14/21 | 7,905.91 | 47115 | 07/23/21 |
| | | 144571 | Umatilla Pedestrian Bridge and Waterline Replacement | 07/14/21 | 927.51 | 47115 | 07/23/21 |
| Total 1089: | | | | | 141,490.42 | | |
| 1099 | | | | | | | |
| 1099 | James C. Fulper dba Health Opt | 95262 | DOT Physical-Bighill | 06/12/21 | 95.00 | 47026 | 07/08/21 |
| Total 1099: | | | | | 95.00 | | |
| 1112 | | | | | | | |
| 1112 | Jimmy's Johns Portable Toilets L | 14565 | Marina & RV Park - Monthly Unit | 07/01/21 | 195.00 | 47116 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------|----------------|--------------------------------|--------------|----------------|--------------|------------------|
| Total 1112: | | | | | 195.00 | | |
| 1189 | | | | | | | |
| 1189 | KIE Supply Corp | 2044245 | Golf Course Irrigation Project | 06/18/21 | 14,546.29 | 46986 | 07/01/21 |
| | | 2046049 | Golf Course Irrigation Project | 06/17/21 | 380.74 | 47117 | 07/23/21 |
| | | 2046200 | Street Dept. Supplies | 06/21/21 | 27.98 | 47029 | 07/08/21 |
| | | 2046714 | Sprinkler Parts | 06/29/21 | 74.57 | 47029 | 07/08/21 |
| | | 2047111 | Water Dept. Supplies | 07/06/21 | 34.56 | 47117 | 07/23/21 |
| | | 2047205 | Sprinkler Parts | 07/07/21 | 11.85 | 47117 | 07/23/21 |
| | | 2047368 | Sprinkler Parts | 07/09/21 | 128.66 | 47117 | 07/23/21 |
| | | 2047472 | Parks Supplies | 07/12/21 | 453.61 | 47117 | 07/23/21 |
| | | 2047697 | Parks Supplies | 07/15/21 | 54.73 | 47117 | 07/23/21 |
| | | 2047748 | Marina Sprinkler Restock | 07/15/21 | 69.81 | 47117 | 07/23/21 |
| Total 1189: | | | | | 15,782.80 | | |
| 1208 | | | | | | | |
| 1208 | Kopacz Nursery & Florist | 6868 | Gardening Supplies | 06/09/21 | 199.90 | Multiple | Multiple |
| | | 6894 | Gardening Supplies | 06/22/21 | 42.95 | Multiple | Multiple |
| | | 6910 | 6th St. Trees | 07/08/21 | 14,400.00 | Multiple | Multiple |
| | | 6910 | Deposit | 07/08/21 | 10,000.00- | 47179 | 07/26/21 |
| Total 1208: | | | | | 4,642.85 | | |
| 1211 | | | | | | | |
| 1211 | Krogh, Theresa | JUNE2021 | Weddings | 07/01/21 | 189.00 | 47030 | 07/08/21 |
| Total 1211: | | | | | 189.00 | | |
| 1224 | | | | | | | |
| 1224 | Kyllo Trucking, Inc. | 211184MCCL | Overpayment Erik McClaenahen | 07/08/21 | 75.00 | 47119 | 07/23/21 |
| Total 1224: | | | | | 75.00 | | |
| 1226 | | | | | | | |
| 1226 | L.N. Curtis and Sons | INV499137 | PD Uniform | 06/16/21 | 500.00 | 47031 | 07/08/21 |
| Total 1226: | | | | | 500.00 | | |
| 1250 | | | | | | | |
| 1250 | League of Oregon Cities | 2021-200394 | Annual Dues | 07/01/21 | 5,957.70 | 47032 | 07/08/21 |
| Total 1250: | | | | | 5,957.70 | | |
| 1257 | | | | | | | |
| 1257 | LEHR | S162736 | OUTFIT POLICE VEHICLE | 06/28/21 | 98.34 | 47033 | 07/08/21 |
| | | S162888 | OUTFIT POLICE VEHICLE | 06/30/21 | 381.83 | 47033 | 07/08/21 |
| | | S162900 | OUTFIT POLICE VEHICLE | 06/30/21 | 406.83 | 47033 | 07/08/21 |
| | | S162919 | OUTFIT POLICE VEHICLE | 07/01/21 | 549.42 | 47033 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|-----------------------------|----------------|-----------------------------------|--------------|----------------|--------------|------------------|
| Total 1257: | | | | | 1,436.42 | | |
| 1263 | | | | | | | |
| 1263 | Les Schwab Tires | 1801205550 | Tires/Repairs | 06/23/21 | 17.24 | 47120 | 07/23/21 |
| | | 1801205550 | Tires/Repairs | 06/23/21 | 17.25 | 47120 | 07/23/21 |
| | | 1801205550 | Tires/Repairs | 06/23/21 | 17.25 | 47120 | 07/23/21 |
| | | 1801205550 | Tires/Repairs | 06/23/21 | 17.25 | 47120 | 07/23/21 |
| | | 1801207794 | Tires/Repairs | 06/30/21 | 17.24 | 47120 | 07/23/21 |
| | | 1801207794 | Tires/Repairs | 06/30/21 | 17.25 | 47120 | 07/23/21 |
| | | 1801207794 | Tires/Repairs | 06/30/21 | 17.25 | 47120 | 07/23/21 |
| | | 1801207794 | Tires/Repairs | 06/30/21 | 17.25 | 47120 | 07/23/21 |
| | | 1801209392 | Tires/Repairs-Golf Course | 07/06/21 | 20.00 | 47120 | 07/23/21 |
| Total 1263: | | | | | 157.98 | | |
| 1297 | | | | | | | |
| 1297 | Lohman, Jason | OAWU2021 | OAWU Conference | 08/22/21 | 319.50 | 47121 | 07/23/21 |
| Total 1297: | | | | | 319.50 | | |
| 1442 | | | | | | | |
| 1442 | MERRITT, GENE | SATURDAY | Reimbursement for Saturday Market | 07/06/21 | 50.00 | 47037 | 07/08/21 |
| Total 1442: | | | | | 50.00 | | |
| 1483 | | | | | | | |
| 1483 | Modern Marketing Inc. | MMI141730 | Library Challenge Wall Calendar | 06/25/21 | 182.86 | 47038 | 07/08/21 |
| Total 1483: | | | | | 182.86 | | |
| 1528 | | | | | | | |
| 1528 | National Geographic Society | 2021SUBSC | Library Subscription | 07/01/21 | 39.00 | 47040 | 07/08/21 |
| Total 1528: | | | | | 39.00 | | |
| 1561 | | | | | | | |
| 1561 | Norco Inc. | 32463044 | Cylinder Rental | 06/30/21 | 45.30 | 47124 | 07/23/21 |
| | | 32463044 | Cylinder Rental | 06/30/21 | 45.30 | 47124 | 07/23/21 |
| Total 1561: | | | | | 90.60 | | |
| 1580 | | | | | | | |
| 1580 | NW Farm Supply Inc. | 2106-105751 | Golf Course Supplies | 06/10/21 | 63.94 | 47041 | 07/08/21 |
| | | 2106-115591 | Golf Course Supplies | 06/29/21 | 159.99 | 47041 | 07/08/21 |
| Total 1580: | | | | | 223.93 | | |
| 1585 | | | | | | | |
| 1585 | O So Kleen, Inc. | H210125WT | City Hall Repairs | 06/24/21 | 499.49 | 47042 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------------|----------------|----------------------------------|--------------|----------------|--------------|------------------|
| Total 1585: | | | | | 499.49 | | |
| 1590 | | | | | | | |
| 1590 | OCEA - Oregon Code Enforcem | MEMBERSHI | Membership Dues-Huxel and Coffey | 07/19/21 | 150.00 | 47126 | 07/23/21 |
| Total 1590: | | | | | 150.00 | | |
| 1636 | | | | | | | |
| 1636 | Oregon Dept of Revenue | JUNE2021 | State Court Assessments | 07/08/21 | 34,704.96 | 47127 | 07/23/21 |
| Total 1636: | | | | | 34,704.96 | | |
| 1645 | | | | | | | |
| 1645 | Oregon Municipal Judges Assn. | 2021DUES | Annual Dues Judge Krogh - 2021 | 07/01/21 | 200.00 | 47128 | 07/23/21 |
| Total 1645: | | | | | 200.00 | | |
| 1652 | | | | | | | |
| 1652 | Oregon Travel Information Coun | 104450 | Oregon State HWY Sign Program | 07/19/21 | 416.00 | 47129 | 07/23/21 |
| Total 1652: | | | | | 416.00 | | |
| 1657 | | | | | | | |
| 1657 | Oriental Trading Company | 710606053-0 | Supplies for Reading Program | 07/02/21 | 26.98 | 47130 | 07/23/21 |
| Total 1657: | | | | | 26.98 | | |
| 1676 | | | | | | | |
| 1676 | OXARC Inc. | 31286255 | Plasma Cutter | 06/18/21 | 1,330.00 | 47044 | 07/08/21 |
| | | 31297455 | Chlorine Cylinders | 06/28/21 | 980.50 | 47132 | 07/23/21 |
| | | 31297456 | Chlorine Cylinders | 06/28/21 | 955.50 | 47132 | 07/23/21 |
| | | 31302965 | Fire Ext. Recharge PD | 07/09/21 | 41.50 | 47132 | 07/23/21 |
| Total 1676: | | | | | 3,307.50 | | |
| 1684 | | | | | | | |
| 1684 | Pacific Power | 0010.07.15.2 | 820 6th St. | 07/15/21 | 63.25 | 47133 | 07/23/21 |
| | | 0013.07.15.2 | Highway 395 & 730 Interti Well | 07/15/21 | 4,211.93 | 47133 | 07/23/21 |
| | | 0021.06.25.2 | WWTP | 06/25/21 | 7,496.07 | 47045 | 07/08/21 |
| | | 0039.06.25.2 | McFarland Well | 06/25/21 | 2,481.98 | 47045 | 07/08/21 |
| | | 0054.06.29.2 | 282 Powerline Rd. | 06/29/21 | 108.49 | 47133 | 07/23/21 |
| | | 0062.07.15.2 | Shop Complex | 07/15/21 | 26.82 | 47133 | 07/23/21 |
| | | 0070.07.15.2 | 8th & F SE Corner | 07/15/21 | 51.10 | 47133 | 07/23/21 |
| | | 0088.07.15.2 | 8th & E St. | 07/15/21 | 22.97 | 47133 | 07/23/21 |
| | | 0096.06.22.2 | 6th & A St. | 06/22/21 | 19.75 | 47045 | 07/08/21 |
| | | 0104.06.25.2 | Street Lights | 06/25/21 | 2,538.53 | 47045 | 07/08/21 |
| | | 0112.07.15.2 | 800 6th St. | 07/15/21 | 68.12 | 47133 | 07/23/21 |
| | | 0112.07.15.2 | 700 6th St. | 07/15/21 | 394.35 | 47133 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------|----------------|-------------------------------|--------------|----------------|--------------|------------------|
| | | 0112.07.15.2 | 700 6th St. | 07/15/21 | 394.35 | 47133 | 07/23/21 |
| | | 0112.07.15.2 | 700 6th St. | 07/15/21 | 394.35 | 47133 | 07/23/21 |
| | | 0120.07.15.2 | 632 D St. Umatilla OR | 07/15/21 | 770.97 | 47133 | 07/23/21 |
| | | 0146.07.15.2 | Bud Draper Dr. | 07/15/21 | 7,614.23 | 47133 | 07/23/21 |
| | | 0153.07.15.2 | Water Booster Station | 07/15/21 | 5,147.50 | 47133 | 07/23/21 |
| | | 0161.07.15.2 | Port Well | 07/15/21 | 7,799.95 | 47133 | 07/23/21 |
| | | 0179.06.25.2 | 285 Radar Rd. | 06/25/21 | 491.58 | 47045 | 07/08/21 |
| | | 0187.07.15.2 | Div 7 Naches Ave. Lift | 07/15/21 | 30.00 | 47133 | 07/23/21 |
| | | 0377.06.22.2 | Bath House Marina | 06/22/21 | 239.62 | 47045 | 07/08/21 |
| | | 0385.06.22.2 | Fish Cleaning Station | 06/22/21 | 19.56 | 47045 | 07/08/21 |
| | | 0393.06.22.2 | West End Comfort Station | 06/22/21 | 23.97 | 47045 | 07/08/21 |
| | | 0401.06.22.2 | 15 HP Pump Marina Levy | 06/22/21 | 363.65 | 47045 | 07/08/21 |
| | | 0419.06.22.2 | Quincy Ave. N 2nd @ Marina | 06/22/21 | 101.14 | 47045 | 07/08/21 |
| | | 0427.06.22.2 | Marina Lights | 06/22/21 | 1,183.44 | 47045 | 07/08/21 |
| | | 0435.06.22.2 | 1710 Quincy St. | 06/22/21 | 243.68 | 47045 | 07/08/21 |
| | | 0443.07.13.2 | Marina Lights | 07/13/21 | 135.26 | 47133 | 07/23/21 |
| | | 0476.06.22.2 | ABT 30322 HWY 730 | 06/22/21 | 25.47 | 47045 | 07/08/21 |
| | | 0500.07.15.2 | 129 Walla Walla St. | 07/15/21 | 37.63 | 47133 | 07/23/21 |
| Total 1684: | | | | | 42,499.71 | | |
| 1715 | | | | | | | |
| 1715 | Pea Ridge Embroidery | 37545 | Embroider Caps | 07/01/21 | 2,419.27 | 47046 | 07/08/21 |
| | | 37580 | Embroider Caps | 07/16/21 | 1,644.50 | 47134 | 07/23/21 |
| Total 1715: | | | | | 4,063.77 | | |
| 1737 | | | | | | | |
| 1737 | Pet Rescue | 215 | Dog Impounds | 07/01/21 | 350.00 | 47048 | 07/08/21 |
| Total 1737: | | | | | 350.00 | | |
| 1763 | | | | | | | |
| 1763 | Platt | Y399433 | Light Bulbs for Library | 06/29/21 | 80.44 | 47049 | 07/08/21 |
| Total 1763: | | | | | 80.44 | | |
| 1791 | | | | | | | |
| 1791 | PRO RENTAL & SALES, INC. | 1564151-000 | Marina Clean-up | 07/02/21 | 1,078.00 | 47050 | 07/08/21 |
| | | 1564331-000 | Streets | 07/02/21 | 331.60 | 47050 | 07/08/21 |
| | | 1567140-000 | Equipment for Parks | 07/13/21 | 148.40 | 47138 | 07/23/21 |
| Total 1791: | | | | | 1,558.00 | | |
| 1818 | | | | | | | |
| 1818 | Quill Corporation | 17889654 | Office Supplies | 07/06/21 | 22.09 | 47139 | 07/23/21 |
| | | 17889654 | Office Supplies | 07/06/21 | 4.39 | 47139 | 07/23/21 |
| | | 17889654 | Office Supplies | 07/06/21 | 8.85 | 47139 | 07/23/21 |
| | | 17889654 | Office Supplies | 07/06/21 | 13.24 | 47139 | 07/23/21 |
| | | 17889654 | Office Supplies | 07/06/21 | 13.24 | 47139 | 07/23/21 |
| | | 17889654 | Office Supplies | 07/06/21 | 8.85 | 47139 | 07/23/21 |
| | | 17889654 | Office Supplies | 07/06/21 | 1.31 | 47139 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------------|----------------|------------------------------------|--------------|----------------|--------------|------------------|
| | | 17901971 | Community Development White Boards | 07/08/21 | 814.95 | 47139 | 07/23/21 |
| Total 1818: | | | | | 886.92 | | |
| 1846 | | | | | | | |
| 1846 | RDO Equipment Co. | P2569765 | John Deere Mower Parts | 07/14/21 | 191.63 | 47141 | 07/23/21 |
| | | W4504465 | Marina Mower | 06/26/21 | 199.28 | 47051 | 07/08/21 |
| Total 1846: | | | | | 390.91 | | |
| 1931 | | | | | | | |
| 1931 | SAIF Corporation | 523883 | W/C Premium/Assment | 07/01/21 | 720.74 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 966.78 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 48.95 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 5,976.50 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 3,536.69 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 18,980.01 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 573.88 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 8,138.71 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 3,703.51 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 4,446.15 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 5,875.38 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 92.10 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 29.63 | 46888 | 07/01/21 |
| | | 523883 | W/C Premium/Assment | 07/01/21 | 1,571.57 | 46888 | 07/01/21 |
| Total 1931: | | | | | 54,660.60 | | |
| 1943 | | | | | | | |
| 1943 | Sanitary Disposal, Inc. | JUNE2021 | Refuse Collection | 07/01/21 | 81,742.75 | 47053 | 07/08/21 |
| | | JUNE2021 | Refuse Collection | 07/01/21 | 13,078.84 | 47053 | 07/08/21 |
| Total 1943: | | | | | 68,663.91 | | |
| 1944 | | | | | | | |
| 1944 | Sanitary Disposal, Transfer St | 713787 | Bio Solids | 06/24/21 | 128.81 | 47146 | 07/23/21 |
| | | 713799 | Bio Solids | 06/24/21 | 135.75 | 47146 | 07/23/21 |
| | | 713812 | Bio Solids | 06/24/21 | 130.09 | 47146 | 07/23/21 |
| | | 713831 | Bio Solids | 06/24/21 | 129.58 | 47146 | 07/23/21 |
| | | 713860 | Bio Solids | 06/24/21 | 145.00 | 47146 | 07/23/21 |
| Total 1944: | | | | | 669.23 | | |
| 1958 | | | | | | | |
| 1958 | Scheel, Leon W. | OAWUCONF | OAWU CONFERENCE 2021 | 08/22/21 | 319.50 | 47147 | 07/23/21 |
| Total 1958: | | | | | 319.50 | | |
| 2013 | | | | | | | |
| 2013 | Signs by Sue | 22672 | Outfit New Police Vehicle | 07/13/21 | 393.00 | 47148 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|-----------------------|----------------|----------------------------|--------------|----------------|--------------|------------------|
| Total 2013: | | | | | 393.00 | | |
| 2059 | | | | | | | |
| 2059 | Smitty's Ace Hardware | 648896 | Water Department supplies | 04/07/21 | 11.17 | 47054 | 07/08/21 |
| | | 650626 | Golf Course Supplies | 05/03/21 | 52.31 | 47054 | 07/08/21 |
| | | 650870 | Parks Supplies | 05/07/21 | 89.80 | 47054 | 07/08/21 |
| | | 653036 | Parks Supplies-Trimmer | 06/11/21 | 429.99 | 47054 | 07/08/21 |
| | | 654086 | Golf Course Supplies | 06/28/21 | 53.98 | 47054 | 07/08/21 |
| | | 654207 | Code Enforcement Supplies | 06/29/21 | 16.36 | 47054 | 07/08/21 |
| | | 654217 | Water Department supplies | 06/22/21 | 34.16 | 47054 | 07/08/21 |
| | | 654258 | Golf Course Supplies | 06/30/21 | 49.54 | 47054 | 07/08/21 |
| | | 654371 | Code Enforcement Supplies | 07/01/21 | 18.57 | 47054 | 07/08/21 |
| | | 654440 | Parks Supplies | 07/02/21 | 47.97 | 47054 | 07/08/21 |
| | | 654816 | Water Department supplies | 07/09/21 | 26.98 | 47150 | 07/23/21 |
| | | 655032 | Golf Course Supplies | 07/13/21 | 33.15 | 47150 | 07/23/21 |
| | | 655074 | Golf Course Supplies | 07/14/21 | 126.19 | 47150 | 07/23/21 |
| | | 655122 | Marina Supplies | 07/15/21 | 117.00 | 47150 | 07/23/21 |
| | | 655212 | Golf Course Supplies | 07/16/21 | 45.97 | 47150 | 07/23/21 |
| Total 2059: | | | | | 1,153.14 | | |
| 2076 | | | | | | | |
| 2076 | Specks Printing | 8456 | Printed Forms and Supplies | 06/28/21 | 55.65 | 47055 | 07/08/21 |
| | | 8456 | Printed Forms and Supplies | 06/28/21 | 11.13 | 47055 | 07/08/21 |
| | | 8456 | Printed Forms and Supplies | 06/28/21 | 22.26 | 47055 | 07/08/21 |
| | | 8456 | Printed Forms and Supplies | 06/28/21 | 3.18 | 47055 | 07/08/21 |
| | | 8456 | Printed Forms and Supplies | 06/28/21 | 33.39 | 47055 | 07/08/21 |
| | | 8456 | Printed Forms and Supplies | 06/28/21 | 33.39 | 47055 | 07/08/21 |
| | | 8480 | Printed Forms and Supplies | 07/07/21 | 126.61 | 47152 | 07/23/21 |
| | | 8480 | Printed Forms and Supplies | 07/07/21 | 25.32 | 47152 | 07/23/21 |
| | | 8480 | Printed Forms and Supplies | 07/07/21 | 50.65 | 47152 | 07/23/21 |
| | | 8480 | Printed Forms and Supplies | 07/07/21 | 7.24 | 47152 | 07/23/21 |
| | | 8480 | Printed Forms and Supplies | 07/07/21 | 75.97 | 47152 | 07/23/21 |
| | | 8480 | Printed Forms and Supplies | 07/07/21 | 75.96 | 47152 | 07/23/21 |
| Total 2076: | | | | | 520.75 | | |
| 2138 | | | | | | | |
| 2138 | SYNCHRONY BANK/AMAZON | 4535986666 | Library programming | 07/17/21 | 500.33 | 47155 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|-------------------------------|----------------|---------------------------------------|--------------|----------------|--------------|------------------|
| | | 4633386466 | Library programming | 07/08/21 | 106.98 | 47155 | 07/23/21 |
| | | 7686746846 | Programming Supplies | 06/16/21 | 34.98 | 47155 | 07/23/21 |
| | | 7969677886 | Program Supplies | 07/08/21 | 38.97 | 47155 | 07/23/21 |
| Total 2138: | | | | | 681.26 | | |
| 2148 | | | | | | | |
| 2148 | Talos Engineering, Inc. | 1779 | IWW Diverson | 06/09/21 | 660.00 | 47058 | 07/08/21 |
| Total 2148: | | | | | 660.00 | | |
| 2158 | | | | | | | |
| 2158 | Taylor, Mike | SLIPRELEA | Slip Release E8 | 06/23/21 | 85.00 | 47059 | 07/08/21 |
| Total 2158: | | | | | 85.00 | | |
| 2217 | | | | | | | |
| 2217 | Tonkon Torp, LLP | 1626439 | Construction Contract Dispute | 06/11/21 | 1,827.00 | 46990 | 07/01/21 |
| Total 2217: | | | | | 1,827.00 | | |
| 2227 | | | | | | | |
| 2227 | Traffic Safety Supply Co. | INV039550 | Stop Signs | 06/25/21 | 248.23 | 47158 | 07/23/21 |
| | | INV38238 | Streets Signage | 05/14/21 | 807.46 | 47061 | 07/08/21 |
| Total 2227: | | | | | 1,055.69 | | |
| 2265 | | | | | | | |
| 2265 | Umatilla Circuit Court | CIT#M17603 | Paid Wrong Court-Alberto Vazquez Carc | 07/08/21 | 115.00 | 47160 | 07/23/21 |
| Total 2265: | | | | | 115.00 | | |
| 2268 | | | | | | | |
| 2268 | Umatilla County Attn: Finance | JUNE2021 | County Court Assessments | 07/08/21 | 3,649.16 | 47161 | 07/23/21 |
| Total 2268: | | | | | 3,649.16 | | |
| 2281 | | | | | | | |
| 2281 | Umatilla Elect. Coop. Assoc. | 4907.07.01.2 | Lights for Waterfall | 07/01/21 | 36.23 | 47162 | 07/23/21 |
| | | 6190.07.01.2 | 60 HP Sewer Pump | 07/01/21 | 40.05 | 47162 | 07/23/21 |
| | | 6190.07.01.2 | Beach Access | 07/01/21 | 70.00 | 47162 | 07/23/21 |
| | | 7216.07.01.2 | 5 HP Sewer Pump | 07/01/21 | 43.59 | 47162 | 07/23/21 |
| | | 7216.07.01.2 | Street Lights | 07/01/21 | 103.58 | 47162 | 07/23/21 |
| Total 2281: | | | | | 293.45 | | |
| 2291 | | | | | | | |
| 2291 | Umatilla School District | EXCISE TAX0 | School District Excise Tax | 07/01/21 | 97,973.47 | 47163 | 07/23/21 |
| Total 2291: | | | | | 97,973.47 | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------------------------------|----------------|---|--------------|----------------|--------------|------------------|
| 2293 | | | | | | | |
| 2293 | Unifirst Corporation | 1430298667 | Bldg Maint/Supplies CH/Library | 06/18/21 | 49.13 | 47063 | 07/08/21 |
| | | 1430298667 | Bldg Maint/Supplies CH/Library | 06/18/21 | 76.11 | 47063 | 07/08/21 |
| | | 1430298667 | Bldg Maint/Supplies CH/Library | 06/18/21 | 76.10 | 47063 | 07/08/21 |
| | | 1430299211 | Bldg Maint/Supplies CH/Library | 06/25/21 | 72.37 | 47063 | 07/08/21 |
| | | 1430299211 | Bldg Maint/Supplies CH/Library | 06/25/21 | 112.11 | 47063 | 07/08/21 |
| | | 1430299211 | Bldg Maint/Supplies CH/Library | 06/25/21 | 112.10 | 47063 | 07/08/21 |
| | | 1430299773 | Bldg Maint/Supplies CH/Library | 07/02/21 | 49.13 | 47063 | 07/08/21 |
| | | 1430299773 | Bldg Maint/Supplies CH/Library | 07/02/21 | 76.11 | 47063 | 07/08/21 |
| | | 1430299773 | Bldg Maint/Supplies CH/Library | 07/02/21 | 76.10 | 47063 | 07/08/21 |
| | | 1430300320 | Bldg Maint/Supplies CH/Library | 07/09/21 | 59.80 | 47164 | 07/23/21 |
| | | 1430300320 | Bldg Maint/Supplies CH/Library | 07/09/21 | 92.65 | 47164 | 07/23/21 |
| | | 1430300320 | Bldg Maint/Supplies CH/Library | 07/09/21 | 92.65 | 47164 | 07/23/21 |
| | | 1430300887 | Bldg Maint/Supplies CH/Library | 07/16/21 | 49.13 | 47164 | 07/23/21 |
| | | 1430300887 | Bldg Maint/Supplies CH/Library | 07/16/21 | 76.11 | 47164 | 07/23/21 |
| | | 1430300887 | Bldg Maint/Supplies CH/Library | 07/16/21 | 76.10 | 47164 | 07/23/21 |
| Total 2293: | | | | | 1,145.70 | | |
| 2295 | | | | | | | |
| 2295 | Uni-Tech Communications Inc. | 14521 | public works- troubleshooting phones | 06/28/21 | 167.00 | 47064 | 07/08/21 |
| | | 14543 | phone system | 07/15/21 | 10,749.42 | 47165 | 07/23/21 |
| | | ESTIMATE64 | Headset Kit | 06/08/21 | 209.20 | 47064 | 07/08/21 |
| | | ESTIMATE64 | Headset Kit | 06/08/21 | 209.19 | 47064 | 07/08/21 |
| Total 2295: | | | | | 11,334.81 | | |
| 2307 | | | | | | | |
| 2307 | UPS | 000084WV82 | PD Postage | 06/26/21 | 18.88 | 47065 | 07/08/21 |
| Total 2307: | | | | | 18.88 | | |
| 2314 | | | | | | | |
| 2314 | USA Bluebook Inc. | 645929 | WWTP Supplies | 06/28/21 | 302.32 | 47066 | 07/08/21 |
| | | 665144 | WWTP Maint. | 07/16/21 | 587.46 | 47166 | 07/23/21 |
| Total 2314: | | | | | 889.78 | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------------|----------------|--------------------------------------|--------------|----------------|--------------|------------------|
| 2337 | | | | | | | |
| 2337 | Verizon Wireless | 9882042631 | Mobile Hotspots | 06/15/21 | 130.51 | 47067 | 07/08/21 |
| | | 9883216652 | Cell Phone Administrator | 07/02/21 | 160.08 | 47169 | 07/23/21 |
| | | 9883216652 | Public Works Phones | 07/02/21 | 131.44 | 47169 | 07/23/21 |
| | | 9883216652 | Cell Phone Administrator | 07/02/21 | 131.44 | 47169 | 07/23/21 |
| | | 9883216652 | Police Cell Phones | 07/02/21 | 1,182.14 | 47169 | 07/23/21 |
| | | 9883216652 | Building Inspector Phone | 07/02/21 | 31.98 | 47169 | 07/23/21 |
| Total 2337: | | | | | 1,767.59 | | |
| 2361 | | | | | | | |
| 2361 | Walmart Community | 7389.06.19.2 | Rec. Program | 06/19/21 | 104.58 | 47069 | 07/08/21 |
| | | 7389.06.19.2 | Rec. Program | 06/19/21 | 91.82 | 47069 | 07/08/21 |
| | | 7389.06.19.2 | Rec. Program | 06/19/21 | 1.48 | 47069 | 07/08/21 |
| | | 7389.06.19.2 | Rec. Program | 06/19/21 | 133.06 | 47069 | 07/08/21 |
| Total 2361: | | | | | 330.94 | | |
| 2452 | | | | | | | |
| 2452 | Withnell Motor Company | 2021CHARG | 2021 Charger VIN#554106 | 06/25/21 | 27,742.91 | 46991 | 07/01/21 |
| | | 2021DURAN | 2021 Dodge Durango VIN#643539 | 06/25/21 | 30,957.22 | 46991 | 07/01/21 |
| Total 2452: | | | | | 58,700.13 | | |
| 2468 | | | | | | | |
| 2468 | Xylem Water Solutions USA, Inc | 3556B79121 | South Hill Waste water pump | 06/29/21 | 18,300.00 | 47176 | 07/23/21 |
| Total 2468: | | | | | 18,300.00 | | |
| 2504 | | | | | | | |
| 2504 | Mount's Lock & Key | 250291 | Locks-Public Works | 07/05/21 | 70.11 | 47123 | 07/23/21 |
| | | 250291 | Locks-Public Works | 07/05/21 | 70.11 | 47123 | 07/23/21 |
| | | 250291 | Locks-Public Works | 07/05/21 | 70.11 | 47123 | 07/23/21 |
| | | 250291 | Locks-Public Works | 07/05/21 | 70.12 | 47123 | 07/23/21 |
| | | 250314 | Marina Locks | 07/09/21 | 517.90 | 47123 | 07/23/21 |
| Total 2504: | | | | | 798.35 | | |
| 2557 | | | | | | | |
| 2557 | Hermiston Ranch & Home | 2106-620454 | Fans for PW Shop | 06/28/21 | 129.99 | 47109 | 07/23/21 |
| | | 2106-620454 | Fans for PW Shop | 06/28/21 | 129.99 | 47109 | 07/23/21 |
| | | 2106-620454 | Fans For Public Works Shop | 06/28/21 | 130.00 | 47109 | 07/23/21 |
| | | 2106-620454 | Fans for PW Shop | 06/28/21 | 130.00 | 47109 | 07/23/21 |
| | | 2106-621383 | Fans For Golf Course | 06/28/21 | 909.95 | 47109 | 07/23/21 |
| Total 2557: | | | | | 1,429.93 | | |
| 2559 | | | | | | | |
| 2559 | Power DMS | INV5694 | Police Dept Accreditation Management | 04/28/21 | 900.00 | 46989 | 07/01/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------|----------------|--|--------------|----------------|--------------|------------------|
| Total 2559: | | | | | 900.00 | | |
| 2564 | | | | | | | |
| 2564 | Rose Hart Pest Control | 31197 | power spray & inject 8 trees for aphid control- Pioneer Cemetery | 06/18/21 | 80.00 | 47052 | 07/08/21 |
| | | 31264 | Aphid control trees at the cemetery | 06/25/21 | 110.00 | 47052 | 07/08/21 |
| | | 31323 | Marina Park | 06/18/21 | 1,500.00 | 47144 | 07/23/21 |
| | | 31551 | Nugent Park | 07/12/21 | 300.00 | 47144 | 07/23/21 |
| Total 2564: | | | | | 1,990.00 | | |
| 2612 | | | | | | | |
| 2612 | Hermiston Transportation | 1207 | Taxi Ticket Redemption | 07/01/21 | 850.00 | 47021 | 07/08/21 |
| Total 2612: | | | | | 850.00 | | |
| 2695 | | | | | | | |
| 2695 | Umpqua Research Company | T004242 | Coliforms | 04/22/21 | 25.00 | 47062 | 07/08/21 |
| | | T004674 | Coliforms | 06/28/21 | 250.00 | 47062 | 07/08/21 |
| Total 2695: | | | | | 275.00 | | |
| 2723 | | | | | | | |
| 2723 | T Mobile | 8369.06.21.2 | Library hotspots | 06/21/21 | 69.70 | 47057 | 07/08/21 |
| Total 2723: | | | | | 69.70 | | |
| 2725 | | | | | | | |
| 2725 | OWLA | FALLCONF | OWLA Fall Conference | 07/20/21 | 150.00 | 47131 | 07/23/21 |
| Total 2725: | | | | | 150.00 | | |
| 2754 | | | | | | | |
| 2754 | GreenPlay LLC | 7297 | Consulting Master Park Plan | 07/10/21 | 603.75 | 47103 | 07/23/21 |
| Total 2754: | | | | | 603.75 | | |
| 2776 | | | | | | | |
| 2776 | OpenGov, Inc. | INV0000461 | OpenGov Implementation/Annual Fee | 07/01/21 | 36,333.00 | 47043 | 07/08/21 |
| Total 2776: | | | | | 36,333.00 | | |
| 2852 | | | | | | | |
| 2852 | City of Umatilla | 6002.07.20.2 | Extra Refuse Marina | 07/01/21 | 803.36 | 47002 | 07/08/21 |
| | | 8092.07.20.2 | Golf Course Refuse | 07/01/21 | 114.80 | 47002 | 07/08/21 |
| Total 2852: | | | | | 918.16 | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|----------------------------------|----------------|--|--------------|----------------|--------------|------------------|
| 2924 | | | | | | | |
| 2924 | Blackstone Publishing | 1230458 | audio books | 06/28/21 | 61.88 | 46994 | 07/08/21 |
| | | 1232582 | audio books | 06/16/21 | 53.85 | 47079 | 07/23/21 |
| | | 1233678 | audio books | 07/16/21 | 26.94 | 47079 | 07/23/21 |
| Total 2924: | | | | | 142.67 | | |
| 2960 | | | | | | | |
| 2960 | Pendleton Bottling Co. | 06302021 | Retail for Golf Course | 06/30/21 | 3.62 | 47047 | 07/08/21 |
| | | 1083652 | Retail for Golf Course | 05/28/21 | 241.60 | 47047 | 07/08/21 |
| | | 10842922 | Retail for Golf Course | 07/02/21 | 69.40 | 47047 | 07/08/21 |
| | | 1084595 | Retail for Golf Course | 06/25/21 | 386.95 | 47047 | 07/08/21 |
| | | 1085038 | Retail for Golf Course | 07/06/21 | 55.20 | 47135 | 07/23/21 |
| | | 1085165 | Retail for Golf Course | 07/09/21 | 218.10 | 47135 | 07/23/21 |
| | | 1085404 | Retail for Golf Course | 07/16/21 | 175.70 | 47135 | 07/23/21 |
| | | 3030319 | Retail for Golf Course Credit | 06/04/21 | 3.00- | 47047 | 07/08/21 |
| | | 3030331 | Retail for Golf Course Credit | 07/16/21 | 18.00- | 47135 | 07/23/21 |
| Total 2960: | | | | | 1,129.57 | | |
| 2971 | | | | | | | |
| 2971 | Talavera Guerrero, Jesus G. | 202395GUE | Overpayment Jesus Talavera Guerrero 202395 | 07/08/21 | 60.00 | 47156 | 07/23/21 |
| Total 2971: | | | | | 60.00 | | |
| 2981 | | | | | | | |
| 2981 | Doug's Septic Service Inc. | 21315 | Portable Toilet-Golf Course | 06/30/21 | 225.00 | 47094 | 07/23/21 |
| Total 2981: | | | | | 225.00 | | |
| 2995 | | | | | | | |
| 2995 | Vern's Food Service Distribution | 144173-00 | Supplies for Golf Course Tournament | 07/07/21 | 365.73 | 47170 | 07/23/21 |
| Total 2995: | | | | | 365.73 | | |
| 3006 | | | | | | | |
| 3006 | Cobra Puma Golf | G2543489 | Golf Course Supplies | 06/01/21 | 63.86 | 47003 | 07/08/21 |
| | | G2543847 | Golf Course Supplies | 06/01/21 | 118.26 | 47003 | 07/08/21 |
| | | G2574651 | Golf Course Retail for Pro Shop | 06/21/21 | 839.62 | 47003 | 07/08/21 |
| | | G2574652 | Golf Course Retail for Pro Shop | 06/22/21 | 31.71 | 47003 | 07/08/21 |
| | | G2574653 | Golf Course Retail for Pro Shop | 06/22/21 | 578.36 | 47003 | 07/08/21 |
| | | G2574654 | Golf Course Retail for Pro Shop | 06/22/21 | 33.90 | 47003 | 07/08/21 |
| | | G2574655 | Golf Course Retail for Pro Shop | 06/01/21 | 274.36 | 47003 | 07/08/21 |
| | | G2581683 | Golf Course Retail for Pro Shop | 06/22/21 | 166.41 | 47003 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|-------------------------------|----------------|---------------------------------------|--------------|----------------|--------------|------------------|
| | | G2587674 | Golf Course Retail for Pro Shop | 06/28/21 | 290.41 | 47088 | 07/23/21 |
| | | G2587675 | Golf Course Retail for Pro Shop | 06/28/21 | 470.69 | 47088 | 07/23/21 |
| | | G2587834 | Golf Course Retail for Pro Shop | 06/28/21 | 82.04 | 47088 | 07/23/21 |
| | | G2587836 | Golf Course Retail for Pro Shop | 06/28/21 | 80.12 | 47088 | 07/23/21 |
| | | G2587922 | Golf Course Retail for Pro Shop | 06/28/21 | 62.04 | 47088 | 07/23/21 |
| | | G2587923 | Golf Course Retail for Pro Shop | 06/28/21 | 74.57 | 47088 | 07/23/21 |
| | | G2596681 | Golf Course Retail for Pro Shop | 07/06/21 | 50.15 | 47088 | 07/23/21 |
| | | G2596857 | Golf Course Retail for Pro Shop | 07/06/21 | 42.04 | 47088 | 07/23/21 |
| | | G2597541 | Golf Course Retail for Pro Shop | 07/07/21 | 53.54 | 47088 | 07/23/21 |
| | | G2597614 | Golf Course Retail for Pro Shop | 07/07/21 | 80.12 | 47088 | 07/23/21 |
| | | G2599465 | Golf Course Supplies | 07/08/21 | 664.06 | 47088 | 07/23/21 |
| | | G2601366 | Golf Course Retail for Pro Shop | 07/12/21 | 120.75 | 47088 | 07/23/21 |
| | | GT2589268 | Golf Course Supplies | 06/30/21 | 76.04 | 47088 | 07/23/21 |
| Total 3006: | | | | | 4,253.05 | | |
| 3019 | | | | | | | |
| 3019 | Pacific Golf & Turf | 03-119959 | Golf Course | 06/11/21 | 35,959.47 | 46987 | 07/01/21 |
| Total 3019: | | | | | 35,959.47 | | |
| 3022 | | | | | | | |
| 3022 | Nakonechny, Lyle | JULY62021 | Archaeological Monitoring-Wanapa Road | 07/06/21 | 12,000.00 | 47039 | 07/08/21 |
| Total 3022: | | | | | 12,000.00 | | |
| 3024 | | | | | | | |
| 3024 | Hodgen Distributing | 234394 | Retail Product for Golf Course | 06/29/21 | 95.56 | 47023 | 07/08/21 |
| | | 234395 | Retail Product for Golf Course | 07/06/21 | 201.60 | 47110 | 07/23/21 |
| | | 235401 | Retail Product for Golf Course | 07/13/21 | 163.46 | 47110 | 07/23/21 |
| Total 3024: | | | | | 460.62 | | |
| 3038 | | | | | | | |
| 3038 | Taylor Made Golf Company Inc. | 34956476 | Merchandise for Pro Shop | 04/22/21 | 515.27 | 47157 | 07/23/21 |
| Total 3038: | | | | | 515.27 | | |
| 3043 | | | | | | | |
| 3043 | DirectTV | 039513239X | TV for Golf Course | 07/22/21 | 111.98 | 47092 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|-------------------------------|----------------|---|--------------|----------------|--------------|------------------|
| Total 3043: | | | | | 111.98 | | |
| 3044 | | | | | | | |
| 3044 | Planet Turf | 21596 | Golf Course | 05/11/21 | 163.00 | 47137 | 07/23/21 |
| | | 22300 | Golf Course | 07/08/21 | 8,475.50 | 47137 | 07/23/21 |
| Total 3044: | | | | | 8,638.50 | | |
| 3049 | | | | | | | |
| 3049 | Creative Signs | 51855 | Library Window Graphics | 06/24/21 | 1,400.00 | 46984 | 07/01/21 |
| Total 3049: | | | | | 1,400.00 | | |
| 3050 | | | | | | | |
| 3050 | James Dean Construction, INC. | PAYREQUES | Wanapa Road and Utilities Extension Project | 07/01/21 | 334,025.70 | 47027 | 07/08/21 |
| Total 3050: | | | | | 334,025.70 | | |
| 3052 | | | | | | | |
| 3052 | Rodriguez, Natalia | 01200826-00 | Reservation Cancellation | 07/13/21 | 74.28 | 47143 | 07/23/21 |
| | | 01200826-00 | Reservation Cancellation | 07/13/21 | 5.72 | 47143 | 07/23/21 |
| | | 01200826-00 | RV Cancellation | 07/09/21 | 74.28 | 47143 | 07/23/21 |
| | | 01200826-00 | RV Cancellation | 07/09/21 | 5.72 | 47143 | 07/23/21 |
| Total 3052: | | | | | 160.00 | | |
| 3058 | | | | | | | |
| 3058 | Wilson, Gene | VEGASTRAI | Travel Reimbursement | 07/21/21 | 2,008.86 | 47173 | 07/23/21 |
| Total 3058: | | | | | 2,008.86 | | |
| 3059 | | | | | | | |
| 3059 | Rider, Rhonda | 01200826-00 | # RV Nights Canceled | 07/06/21 | 481.52 | 47142 | 07/23/21 |
| | | 01200826-00 | # RV Nights Canceled | 07/06/21 | 38.48 | 47142 | 07/23/21 |
| | | 01200826-00 | RV Park Cancellation | 07/06/21 | 296.32 | 47142 | 07/23/21 |
| | | 01200826-00 | RV Park Cancellation | 07/06/21 | 23.68 | 47142 | 07/23/21 |
| Total 3059: | | | | | 840.00 | | |
| 3062 | | | | | | | |
| 3062 | Northwest Golf Cars | 16956K | Tournament Fleet Rentals | 07/03/21 | 2,620.00 | 47125 | 07/23/21 |
| Total 3062: | | | | | 2,620.00 | | |
| 3071 | | | | | | | |
| 3071 | Portable Storage Rentals | 1433 | Deposit on Golf Cart Sheds | 06/17/21 | 8,460.00 | 46988 | 07/01/21 |
| | | 1433 | Deposit on Golf Cart Sheds | 06/17/21 | 47,940.00 | | |
| Total 3071: | | | | | 56,400.00 | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------------------------|----------------|---------------------------------|--------------|----------------|--------------|------------------|
| 3072 | | | | | | | |
| 3072 | Maganas, Alonzo | 01200626-00 | Reservation Cancellation | 06/29/21 | 111.42 | 47034 | 07/08/21 |
| | | 01200626-00 | Reservation Cancellation | 06/29/21 | 8.58 | 47034 | 07/08/21 |
| Total 3072: | | | | | 120.00 | | |
| 3073 | | | | | | | |
| 3073 | Mejia Ortega, Hazael | CDL2021 | Reimbursement for CDL | 07/01/21 | 138.00 | 47036 | 07/08/21 |
| Total 3073: | | | | | 138.00 | | |
| 3074 | | | | | | | |
| 3074 | Crow, Bob | 01210417-00 | Reservation Cancellation | 07/01/21 | 501.45 | 47005 | 07/08/21 |
| | | 01210417-00 | Reservation Cancellation | 07/01/21 | 40.05 | 47005 | 07/08/21 |
| Total 3074: | | | | | 541.50 | | |
| 3075 | | | | | | | |
| 3075 | Garber, Tim | E14SLIPREL | Slip Release | 06/27/21 | 85.00 | 47014 | 07/08/21 |
| Total 3075: | | | | | 85.00 | | |
| 3076 | | | | | | | |
| 3076 | Jarvis, Rene | 01210322-00 | Reservation Cancellation | 07/01/21 | 100.29 | 47028 | 07/08/21 |
| | | 01210322-00 | Reservation Cancellation | 07/01/21 | 8.01 | 47028 | 07/08/21 |
| Total 3076: | | | | | 108.30 | | |
| 3077 | | | | | | | |
| 3077 | McKinney, James | SLIPRELEA | slip release and deposit | 06/30/21 | 196.67 | 47035 | 07/08/21 |
| Total 3077: | | | | | 196.67 | | |
| 3078 | | | | | | | |
| 3078 | Sundet, Samuel | 01200826-00 | Refund for cancellation | 07/15/21 | 74.08 | 47154 | 07/23/21 |
| | | 01200826-00 | Refund for cancellation | 07/15/21 | 5.92 | 47154 | 07/23/21 |
| | | 01200826-00 | Refund for cancellation | 07/09/21 | 74.08 | 47154 | 07/23/21 |
| | | 01200826-00 | Refund for cancellation | 07/09/21 | 5.92 | 47154 | 07/23/21 |
| | | 01200826-00 | Refund for cancellation | 06/24/21 | 74.28 | 47056 | 07/08/21 |
| | | 01200826-00 | Refund for cancellation | 06/24/21 | 5.72 | 47056 | 07/08/21 |
| Total 3078: | | | | | 240.00 | | |
| 3079 | | | | | | | |
| 3079 | Walchli, Janice | FIREWOOD2 | Firewood for Marina/RV Park | 07/01/21 | 480.00 | 47068 | 07/08/21 |
| Total 3079: | | | | | 480.00 | | |
| 3080 | | | | | | | |
| 3080 | The Monster Mural LLC. | PO15967 | Programing - National Night Out | 07/06/21 | 1,549.00 | 47060 | 07/08/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|-------------------------------|----------------|--|--------------|----------------|--------------|------------------|
| Total 3080: | | | | | 1,549.00 | | |
| 3081 | | | | | | | |
| 3081 | Good Sam Campground & Coup | 2022EDITIO | Campground Guide | 07/12/21 | 9,093.12 | 47100 | 07/23/21 |
| Total 3081: | | | | | 9,093.12 | | |
| 3082 | | | | | | | |
| 3082 | Abboud, Tarek | ABBOUD211 | Overpayment-211200 Tarek Abboud | 07/08/21 | 300.00 | 47074 | 07/23/21 |
| Total 3082: | | | | | 300.00 | | |
| 3083 | | | | | | | |
| 3083 | William, Keith J. | WILLIAM210 | Overpayment 210833 Keith J. William | 07/08/21 | 200.00 | 47172 | 07/23/21 |
| Total 3083: | | | | | 200.00 | | |
| 3084 | | | | | | | |
| 3084 | Singh, Inder P. | 211186SING | Overpayment 211186 Inder P. Singh | 07/08/21 | 215.00 | 47149 | 07/23/21 |
| Total 3084: | | | | | 215.00 | | |
| 3085 | | | | | | | |
| 3085 | Fraser, Bryan | 211235CAPE | Overpayment Steve Capetillo 211235 | 07/08/21 | 215.00 | 47098 | 07/23/21 |
| Total 3085: | | | | | 215.00 | | |
| 3086 | | | | | | | |
| 3086 | Rains, Robert D. | 210148RAIN | Overpayment Rains 210148 | 07/08/21 | 85.00 | 47140 | 07/23/21 |
| Total 3086: | | | | | 85.00 | | |
| 3087 | | | | | | | |
| 3087 | ASC EDPRESS LLC | 211086BIRD | Overpayment Larry E. Bird 211086 | 07/08/21 | 25.00 | 47076 | 07/23/21 |
| Total 3087: | | | | | 25.00 | | |
| 3088 | | | | | | | |
| 3088 | VIP Cargo Express LLC | 211245GHE | Overpayment Lilian M. Gherasim 211245 | 07/08/21 | 215.00 | 47171 | 07/23/21 |
| Total 3088: | | | | | 215.00 | | |
| 3089 | | | | | | | |
| 3089 | Velazquez Gutierrez, Maria C. | 191801GUE | Overpayment Maria Velazquez Gutierrez 191801 | 07/08/21 | 60.00 | 47168 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|---------------------------|----------------|--|--------------|----------------|--------------|------------------|
| Total 3089: | | | | | 60.00 | | |
| 3090 | | | | | | | |
| 3090 | Barreto, Guadalupe | 201532FLOR | Overpayment Sandra Aguilar Flores 201532 | 07/08/21 | 10.00 | 47078 | 07/23/21 |
| Total 3090: | | | | | 10.00 | | |
| 3091 | | | | | | | |
| 3091 | 49er Trucking | 210994PANI | Overpayment Anibal Estrada Paniag | 07/08/21 | 215.00 | 47070 | 07/23/21 |
| Total 3091: | | | | | 215.00 | | |
| 3092 | | | | | | | |
| 3092 | Cobb, Ronald F. | 211101COPE | Overpayment Alan R. Cope 211101 | 07/08/21 | 25.00 | 47087 | 07/23/21 |
| Total 3092: | | | | | 25.00 | | |
| 3093 | | | | | | | |
| 3093 | IMCO General Construction | 211189SCHE | 211189 William Scheffer | 07/08/21 | 225.00 | 47112 | 07/23/21 |
| Total 3093: | | | | | 225.00 | | |
| 3094 | | | | | | | |
| 3094 | South Pacific Transport | 211322 | Overpayment 211322 | 07/08/21 | 215.00 | 47151 | 07/23/21 |
| Total 3094: | | | | | 215.00 | | |
| 3095 | | | | | | | |
| 3095 | Woods, Tony C. | 211344WOO | Overpayment Tony Woods 211344 | 07/08/21 | 215.00 | 47175 | 07/23/21 |
| Total 3095: | | | | | 215.00 | | |
| 3096 | | | | | | | |
| 3096 | SRD Transportation | 211438TONE | Overpayment Iliya G Tonev 211438 | 07/08/21 | 215.00 | 47153 | 07/23/21 |
| Total 3096: | | | | | 215.00 | | |
| 3097 | | | | | | | |
| 3097 | Triad Machinery | 211440KUCI | Overpayment Robert J. Kuciej 211440 | 07/08/21 | 215.00 | 47159 | 07/23/21 |
| Total 3097: | | | | | 215.00 | | |
| 3098 | | | | | | | |
| 3098 | Zoric, James R. | 211456ZORI | Overpayment James R. Zoric | 07/08/21 | 50.00 | 47178 | 07/23/21 |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|--------------------------|----------------|-----------------------------------|--------------|----------------|--------------|------------------|
| Total 3098: | | | | | 50.00 | | |
| 3099 | | | | | | | |
| 3099 | Marka Transport | 211514HOLD | Overpayment Oleg M. Holden 211514 | 07/08/21 | 215.00 | 47122 | 07/23/21 |
| Total 3099: | | | | | 215.00 | | |
| 3100 | | | | | | | |
| 3100 | Ross, Marie | 070821ROS | Wilderness Camp Refund | 07/08/21 | 117.50 | 47145 | 07/23/21 |
| Total 3100: | | | | | 117.50 | | |
| 3101 | | | | | | | |
| 3101 | Zoll Medical Corporation | 3315425 | Battery-IED | 07/05/21 | 1,221.17 | 47177 | 07/23/21 |
| | | 3316699 | Electrodes, Adult | 07/06/21 | 429.29 | 47177 | 07/23/21 |
| Total 3101: | | | | | 1,650.46 | | |
| 3102 | | | | | | | |
| 3102 | Conrad, William | 78261681 | Hydrant Rental Deposit Refund | 07/19/21 | 1,348.60 | 47089 | 07/23/21 |
| Total 3102: | | | | | 1,348.60 | | |
| 3103 | | | | | | | |
| 3103 | Hanson, Scott | KEYDEPOS | Key Deposit Refund | 07/05/21 | 10.00 | 47105 | 07/23/21 |
| Total 3103: | | | | | 10.00 | | |
| 3104 | | | | | | | |
| 3104 | Bradley, Val | 07.2021 | Reimbursement for clean up | 07/21/21 | 105.42 | 47081 | 07/23/21 |
| Total 3104: | | | | | 105.42 | | |
| 3105 | | | | | | | |
| 3105 | Vega, Carmen | 4206THST | Final Utility Bill-Refund | 07/21/21 | 51.93 | 47167 | 07/23/21 |
| Total 3105: | | | | | 51.93 | | |
| 3106 | | | | | | | |
| 3106 | Peralta, Javier | 100DARKDA | Utility Refund | 07/21/21 | 100.00 | 47136 | 07/23/21 |
| Total 3106: | | | | | 100.00 | | |
| 3107 | | | | | | | |
| 3107 | Gonzalez, Monica | 01200826-00 | Reservation Cancellation | 07/19/21 | 74.28 | 47099 | 07/23/21 |
| | | 01200826-00 | Reservation Cancellation | 07/19/21 | 5.72 | 47099 | 07/23/21 |
| Total 3107: | | | | | 80.00 | | |

| Vendor Number | Name | Invoice Number | Description | Invoice Date | Invoice Amount | Check Number | Check Issue Date |
|---------------|------------|----------------|--------------|--------------|----------------|--------------|------------------|
| 3108 | | | | | | | |
| 3108 | Woods, Tim | SLIPRELEA | Slip Release | 07/16/21 | 553.77 | 47174 | 07/23/21 |
| Total 3108: | | | | | 553.77 | | |
| Grand Totals: | | | | | 1,361,462.35 | | |

Report Criteria:
Detail report type printed

CITY OF UMATILLA, OREGON

AGENDA BILL

| | |
|--------------------------------------|------------------------------------|
| Agenda Title: Court Report | Meeting Date: 2021-08-02 |
|--------------------------------------|------------------------------------|

| | | | |
|---|-------------------------------------|---|----------------------|
| Department: Finance & Administrative Services | Director: David Stockdale | Contact Person: Theresa Krogh | Phone Number: |
|---|-------------------------------------|---|----------------------|

| | |
|--------------------------------|---|
| Cost of Proposal: NA | Fund(s) Name and Number(s): N/A |
| Amount Budgeted: NA | |

| | |
|--|------------------------------------|
| Reviewed by Finance Department: No | Previously Presented: NA |
|--|------------------------------------|

Attachments to Agenda Packet Item:

[June 2021 Court Report.pdf](#)

[May 2021 Court Report.pdf](#)

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|--|
| Summary Statement: Approve May & April |
|--|

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|---|
| Consistent with Council Goals: Goal 4: Increase Public Involvement, Create a Culture of Transparency with the Public, and Enhance Cultural Diversity. |
|---|



Umatilla Municipal Court

700 6th Street, PO Box 130, Umatilla, OR 97882
(541) 922-4570 Ext. 2 Fax (541) 922-5758

JULY 8, 2021

TO: MAYOR MARY DEDRICK
FROM: THERESA M KROGH , MUNICIPAL JUDGE
SUBJECT: JUNE 2021 MONTH END REPORT

| <u>TOTAL</u> | <u>POLICE TRAINING COUNTY</u> | | <u>STATE</u> | <u>REFUNDS</u> |
|---------------------|-------------------------------|----------------|-------------------|--------------------|
| <u>RECEIPTS</u> | <u>FINES</u> | <u>FUND</u> | <u>ASSESS..</u> | <u>ASSESS.</u> |
| <u>\$ 73,431.69</u> | <u>\$31,900.07</u> | <u>\$ 0.00</u> | <u>\$3,649.16</u> | <u>\$34,704.96</u> |
| | | | | <u>\$3,165.00</u> |

REFUNDS INCLUDES, OVERPAYMENTS, RESTITUTION, REFUNDS TO DEFENDANTS AND BAIL TRANSFERRED TO OTHER COURTS.

TURNOVER FOR JUNE 2021 IS \$31,900.07

THREE WEDDING WERE PERFORMED IN JUNE 2021 OUT OF THE COURTROOM.

Theresa M Krogh
Theresa M Krogh
Municipal Judge for
The City of Umatilla

:tmk

CC: COUNCIL



Umatilla Municipal Court

700 6th Street, PO Box 130, Umatilla, OR 97882
(541) 922-4570 Ext.2 Fax (541) 922-5758

JUNE 17, 2021

TO: MAYOR MARY DEDRICK
FROM: THERESA M KROGH , MUNICIPAL JUDGE
SUBJECT: MAY 2021 MONTH END REPORT

| <u>TOTAL</u> | <u>POLICE TRAINING COUNTY</u> | | | <u>STATE</u> | <u>REFUNDS</u> |
|---------------------|-------------------------------|----------------|-------------------|--------------------|-------------------|
| <u>RECEIPTS</u> | <u>FINES</u> | <u>FUND</u> | <u>ASSESS..</u> | <u>ASSESS.</u> | |
| \$ 61,203.27 | \$26,658.75 | \$ 0.00 | \$3,329.34 | \$29,984.40 | \$1,218.28 |

REFUNDS INCLUDES, OVERPAYMENTS, RESTITUTION, REFUNDS TO DEFENDANTS AND BAIL TRANSFERRED TO OTHER COURTS.

TURNOVER FOR MAY 2021 IS \$26,658.75

TWO WEDDING WAS PERFORMED IN MAY 2021 OUT OF THE COURTROOM.

Theresa M Krogh
Theresa M Krogh
Municipal Judge for
The City of Umatilla

:tmk

CC: COUNCIL

CITY OF UMATILLA, OREGON

AGENDA BILL

| | |
|---|------------------------------------|
| Agenda Title: Cleaver Land Plan Amendment PA-2-20: The applicant, Cleaver Land, LLC, is requesting approval of an Urban Growth Boundary Expansion to include approximately 146.69 acres land. The applicant also submitted an Annexation and Zone Change applications with the desired outcome to have approximately 450 acres of land planned and zoned for industrial use. | Meeting Date: 2021-08-02 |
|---|------------------------------------|

| | | | |
|---|-----------------------------------|---|----------------------|
| Department: Community Development | Director: Brandon Seitz | Contact Person: Brandon Seitz | Phone Number: |
|---|-----------------------------------|---|----------------------|

| | |
|--------------------------------|---|
| Cost of Proposal: NA | Fund(s) Name and Number(s): N/A |
| Amount Budgeted: NA | |

| | |
|---|------------------------------------|
| Reviewed by Finance Department: Yes | Previously Presented: NA |
|---|------------------------------------|

Attachments to Agenda Packet Item:

[PA-2-20 CC Report \(Aug 2nd\).pdf](#)

| |
|---|
| Summary Statement: The Umatilla City Council continued Cleaver Land Plan Amendment PA-2-20 at their July 20, 2021 council meeting and left the record open. The findings and recommendation for Cleaver Plan Amendment PA-2-20 are attached. A copy of the findings and all exhibits are available on the City's website at the link below or are available at City Hall during normal business hours. Cleaver Land Plan Amendment PA-2-20 A sample motion for an approval of Cleaver Land Plan Amendment PA-2-20 is provided below. I move to approve Cleaver Land Plan Amendment PA-2-20 and adopt the findings and recommendation for Cleaver Plan Amendment PA-2-20 as the Councils own. |
|---|

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|---|
| Consistent with Council Goals: Goal 2: Promote Economic Development and Job Growth. |
|---|

**UMATILLA CITY COUNCIL
REPORT AND RECOMMENDATION
FOR
PLAN AMENDMENT PA-2-20**

DATE OF HEARING: August 2, 2021

REPORT PREPARED BY: Brandon Seitz, Community Development Director

I. GENERAL INFORMATION AND FACTS

Applicant: Cleaver Land, LLC, 78757 Westland Rd, Hermiston, OR 97838

Land Use Review: An Urban Growth Boundary (UGB) expansion.

II. NATURE OF REQUEST AND GENERAL FACTS

The applicant, Cleaver Land, LLC, is requesting approval of an Urban Growth Boundary Expansion to include approximately 146.69 acres land. The applicant also submitted an Annexation and Zone Change applications with the desired outcome to have approximately 450 acres of land planned and zoned for industrial use. Current use of the property is agricultural. Crops under circle pivot irrigation regularly in rotation are potatoes, onions, corn, and grass seed. Improvements to the property include circle pivot irrigation systems and a general use storage building.

Applicants Intended Outcomes of Application Process:

The applicant is working with the City of Umatilla to achieve approval of three applications – an Urban Growth Boundary (UGB) expansion, an Annexation, and a Zone Change – with the desired outcome to have some 450 acres of available land planned and zoned for industrial use. The UGB expansion will add about 150 acres to the UGB; the Annexation will add those same acres within the City Limits; and those actions combined with a Zone Change will add about 450 acres to the industrial land supply. The proposed zoning designation of Light Industrial will support the types of uses – data centers, warehousing and light manufacturing – outlined in the Economic Opportunities Analysis completed by Johnson Economics that indicates that the City of Umatilla is in need of large lot industrial parcels. On page 43 of the Economic Opportunities Analysis it states, “For industrial users, there is an estimated deficit of sites of some sizes. Most notably there is a deficit of suitable large industrial sites, and a deficit of small industrial sites.” This statement is expanded on pages 44 and 45 providing more definition to the needs. At the top of page 45 the report states, “Given the projected short-term growth, and prospective long-term growth in this industry [data centers], Johnson Economics estimates a need for at least one site of 100+ acres meeting serviceability requirements for data center or large manufacturing users, and at least two sites of 50+ acres.” Johnson Economics also states on page 41 the following, “...this does not address the more specific site needs from specific categories of employment land users. Some of the forecasted growth includes employers who may have specific site needs and preferences that are not reflected in the available buildable inventory, even though *in total* the

available parcels sum to a significant amount. In particular, there is forecasted demand for more suitable large-lot industrial sites while relatively few of these sites were found in the inventory.” The Johnson Economics provided Economic Opportunities Analysis, while using acreage ranges to discuss needs, does acknowledge that needs for large lots over 100 acres might easily mean upwards of 200 acres for any single user. Examples are a data center request at more than 120 acres and the Walmart Distribution Center at 190 acres. This would also be applicable to the range of 50 to 99.9 acres which could result in users needing 65 acres or 92 acres, an example being the FedEx freight distribution facility at 62.5 acres.

This suite of applications seeks to add 450 acres to the industrial land inventory for the City of Umatilla, meeting this need with the ability to also meet future needs for smaller lot or clustered industrial development which is also identified as a need. The Johnson Economics report on page 45 states the following about small lots, “There is also a projected need from small industrial firms for smaller sites. It is also common for these types of users to also be accommodated in multi-tenant industrial buildings on larger sites.”

The zone change component of this suite of applications does propose to rezone approximately 300 acres from Residential to Industrial. In 2019 the City of Umatilla completed a Goal 10 update that included a buildable lands inventory and a Housing Strategies Report (2019) that indicates an overabundance of residential land. Removal of 300 acres of residential land from the inventory does not negatively impact the land supply for residential development in the 20-year planning period, leaving a continuing surplus of approximately 750 acres.

III. ANALYSIS

The criteria applicable to this request are shown in underlined text and the responses are shown in standard text. All of the following criteria must be satisfied in order for this request to be approved.

Oregon Administrative Rule Chapter 660 Division 24 Section 0020 Adoption or Amendment of a UGB identifies which Statewide Planning Goals and related administrative rules are applicable. The following are considered:

(1) All statewide goals and related administrative rules are applicable when establishing or amending a UGB, except as follows:

(a) The exceptions process in Goal 2 and OAR chapter 660, division 4, is not applicable unless a local government chooses to take an exception to a particular goal requirement, for example, as provided in OAR 660-004-0010(1);

Applicants Response: Based on the provisions outlined here no exceptions to any of the Statewide Planning Goals are necessary. Later analyzed are additional Division 24 requirements meeting current planning requirements for an urban growth boundary expansion.

Conclusion: The City is not claiming a goal exception.

(b) Goals 3 and 4 are not applicable;

Applicants Response: The applicant is relying on the Economic Opportunities Analysis (October

2019) which utilizes Oregon Administrative Rule Chapter 660 Division 24. This allows for an application to expand the urban growth boundary without an exception to Goal 3 Agricultural Land. The land under consideration for this urban growth boundary expansion is zoned Exclusive Farm Use and is currently inventoried in Umatilla County as part of Goal 3 protected lands. This action would remove approximately 150 acres from that inventory, adding it to the City of Umatilla urban growth boundary and city limits (by way of the included annexation application if approved).

Conclusion: Expansion of the urban growth boundary is allowed without an exception to State Goal 3 by way of Oregon Administrative Rule Chapter 660 Division 24. Goal 4 is not applicable as there are no Forest Lands found in or surrounding the City of Umatilla.

(c) Goal 5 and related rules under OAR chapter 660, division 23, apply only in areas added to the UGB, except as required under OAR 660-023-0070 and 660-023-0250;

Applicants Response: The applicant, based on conversations with City of Umatilla staff, is not aware of or has identified any Goal 5 resources within the subject property for either the urban growth boundary expansion and associated annexation or within the area proposed to be zoned or rezoned to Light Industrial.

Conclusion: According to the City of Umatilla Comprehensive Plan there are no identified Goal 5 resources on the subject property. The proposed urban growth boundary expansion, associated annexation, and rezone would not affect any known Goal 5 resources.

(d) The transportation planning rule requirements under OAR 660-012-0060 need not be applied to a UGB amendment if the land added to the UGB is zoned as urbanizable land, either by retaining the zoning that was assigned prior to inclusion in the boundary or by assigning interim zoning that does not allow development that would generate more vehicle trips than development allowed by the zoning assigned prior to inclusion in the boundary;

Applicants Response: The included Traffic Impact Analysis finds that the conversion of the residential land to industrial creates a reduction in trips. For the land that is subject to the urban growth boundary expansion and annexation, approximately 150 acres, there would be an increase in traffic over current Exclusive Farm Use zoning. That increase is consumed by the change in zoning of nearly 300 acres with a decrease in total daily trips. Transportation impacts are further analyzed later in this narrative and are evaluated in the included Traffic Impact Analysis.

Conclusion: A decrease in total daily trips is the net result from all three applications. Transportation impacts are further analyzed later in this narrative.

(e) Goal 15 is not applicable to land added to the UGB unless the land is within the Willamette River Greenway Boundary;

Applicants Response: The City of Umatilla is not within the Willamette River Greenway Boundary. Goal 15 is not considered here or elsewhere in this narrative.

Conclusion: The City of Umatilla is not within the Willamette River Greenway Boundary. Goal 15 is not applicable.

(f) Goals 16 to 18 are not applicable to land added to the UGB unless the land is within a coastal shorelands boundary;

Applicants Response: The City of Umatilla is not within a coastal shorelands boundary. Goals 16 through 18 are not considered here or elsewhere in this narrative.

Conclusion: The City of Umatilla is not within a coastal shorelands boundary. Goals 16 through 18 are not applicable.

(g) Goal 19 is not applicable to a UGB amendment.

Applicants Response: Goal 19 is not considered here or elsewhere in this narrative.

Conclusion: Goal 19 is not applicable.

(2) The UGB and amendments to the UGB must be shown on the city and county plan and zone maps at a scale sufficient to determine which particular lots or parcels are included in the UGB. Where a UGB does not follow lot or parcel lines, the map must provide sufficient information to determine the precise UGB location.

Applicants Response: Maps are included as part of the application package. The area subject to the urban growth boundary expansion (and related annexation) is the portion of Tax Lot 1400 of Assessor's Map 5N28C that is outside of the current urban growth boundary and city limit line. Also included is Tax Lot 6601 of Assessor's Map 5N28C. The acreage of the urban growth boundary expansion is approximately 150 acres. The Powerline Road right-of-way is also included in the urban growth boundary expansion to facilitate the future transfer of the portion of the Road from Umatilla County to the City of Umatilla.

Conclusion: The maps included in the application package clearly show the intention of the application. They are all at a scale sufficient to determine which particular lots or parcels are included in the UGB and subsequent applications.

Applicants Note: As part of the Economic Opportunities Analysis, Johnson Economics evaluated Oregon Administrative Rule Chapter 660 Division 24 Section 0040 Land Need and Section 0050 Land Inventory and Response to Deficiency. See pages 28 through 36 of the Economic Opportunities Analysis for evaluation and analysis of these two sections of OAR 660 Division 24.

Oregon Administrative Rule Chapter 660 Division 24 Section 0065 Establishment of Study Area to Evaluate Land for Including in the UGB is a continuation of the work embodied in the included Economic Opportunities Analysis which determines a need for large lot industrial opportunities. As part of the Economic Opportunities Analysis, Johnson Economics evaluated Oregon Administrative Rule Chapter 660 Division 24 Section 0040 Land Need and Section 0050 Land Inventory and Response to Deficiency. Section 0065 is reviewed here:

(1) When considering a UGB amendment to accommodate a need deficit identified in OAR 660-024-0050(4), a city outside of Metro must determine which land to add to the UGB by evaluating alternative locations within a “study area” established pursuant to this rule. To establish the study area, the city must first identify a “preliminary study area” which shall not include land within a different UGB or the corporate limits of a city within a different UGB. The preliminary study area shall include:

(a) All lands in the city’s acknowledged urban reserve, if any;

Applicants Response: The City of Umatilla does not have an acknowledged urban reserve. This is not applicable.

Conclusion: The City of Umatilla does not have an acknowledged urban reserve. This is not applicable.

(b) All lands that are within the following distance from the acknowledged UGB:

(A) For cities with a UGB population less than 10,000: one-half mile;

(B) For cities with a UGB population equal to or greater than 10,000: one mile;

(c) All exception areas contiguous to an exception area that includes land within the distance specified in subsection (b) and that are within the following distance from the acknowledged UGB:

(A) For cities with a UGB population less than 10,000: one mile;

(B) For cities with a UGB population equal to or greater than 10,000: one and one-half miles;

(d) At the discretion of the city, the preliminary study area may include land that is beyond the distance specified in subsections (b) and (c).

Applicants Response: The applicant, working with City staff, originally identified three sites to evaluate as alternatives to the subject property. These three Sites along with the subject property are identified on maps included with this application package.

Based on comment from the Department of Land Conservation and Development additional lands have been included and are identified in the tables below. The three alternative sites that are most fully analyzed are: 1) land to the east of the Port of Umatilla development and north of Highway 730 along the banks of the Columbia River (Site 1), 2) land east and south of the Port of Umatilla and both north and south of Highway 730 (Site 2), and 3) land to the south of the City of Umatilla between Highway 395 and Interstate 82 (Site 3). The subject property is along Powerline Road to the south of the City of Umatilla. It should be noted that to the north of the City of Umatilla is the Columbia River and the State of Washington thereby restricting expansion and development.

The following sites are within the city limits and over 90 acres:

| Tax Account # | Map & tax lot | OWNER | Acreage | Current Use |
|---------------|---------------|----------------------------|---------|-------------|
| 133088 | 5N28210000200 | AMAZON DATA SERVICES INC | 178.2 | Data Center |
| 135855 | 5N29B00000600 | PORT OF UMATILLA | 161.36 | Empty land |
| 135832 | 5N28A00000101 | OREGON DEPT OF CORRECTIONS | 268.15 | Prison |

| | | | | |
|----------------------|---|---------------------------------|--------|------------------|
| <u>124632</u> | <u>5N28C0000140</u> <u>0</u> | <u>CLEAVER LAND, LLC</u> | 214 | Subject Property |
| <u>124632</u> | <u>5N28C0000140</u> <u>0</u> | <u>CLEAVER LAND, LLC</u> | 106.34 | Subject Property |

Based on discussion with City of Umatilla staff the Amazon data center site is under development, the Port of Umatilla property is under consideration for development at the time of application, and the Oregon Department of Corrections property, while partially vacant, is considered unavailable for development. The property owned by Cleaver Land is the subject property.

The following sites are within the study area and are generally over 90 acres:

| Tax Account # | Map & tax lot | OWNER | Acreage | Current Use |
|---------------|---------------|--------------------------------|---------|--------------|
| 128455 | 5N28C00001300 | TOPAZ LAND INC | 635.74 | Agriculture |
| 129006 | 5N27000000401 | N & C LAND LLC | 432.44 | Agriculture |
| 124666 | 5N28C00006701 | TOPAZ LAND INC | 319.89 | Agriculture |
| 128459 | 5N28C00001401 | TOPAZ LAND INC | 155.45 | Agriculture |
| 158438 | 5N28330000200 | BROKEN SPUR RANCH LLC | 106.56 | Agriculture |
| 133096 | 5N28C00001200 | TOPAZ LAND INC | 595.5 | Agriculture |
| 129011 | 5N27000000501 | N & C LAND LLC | 594.29 | Agriculture |
| 148171 | 5N28C00001404 | BROKEN SPUR RANCH LLC | 135.4 | Agriculture |
| 127025 | 5N29B00000203 | DEPT OF INTERIOR BIA | 713.88 | Federal Land |
| 150061 | 5N29B00000601 | USA Site 2 | 479.15 | Federal Land |
| 133108 | 5N28230000100 | DEPT OF INTERIOR BIA Site 2 | 318 | Federal Land |
| 126982 | 5N28240000100 | DEPT OF INTERIOR BIA Site 2 | 200.59 | Federal Land |
| 127030 | 5N29B00000301 | OREGON DEPT FISH & WILDLIFE | 160 | State Land |
| 135854 | 5N29B00000400 | USA | 102.31 | Federal Land |
| 136210 | 5N2828C000200 | USA | 95.76 | Federal Land |
| 126980 | 5N28A00001300 | DEPT OF INTERIOR BIA Site 2 | 465.36 | Federal Land |
| 136246 | 5N28A00000400 | USA | 659.59 | Federal Land |
| 136258 | 5N28090000100 | USA | 256.17 | Federal Land |
| 127039 | 5N29B00000500 | USA (TRS) Site 1 | 195.23 | Federal Land |
| 135814 | 5N28A00000100 | USA | 134.98 | Federal Land |
| 136249 | 5N28140001600 | USA | 105.21 | Federal Land |
| 136324 | 5N28180000601 | USA | 95.1 | Federal Land |
| 136228 | 5N27130001001 | USA | 90.82 | Federal Land |
| 136211 | 5N2828A000100 | USA (BLM) Site 3 | 77.43 | Federal Land |

| | | | | |
|----------------------|-----------------------------|---------------------------------|--------|-----------------------------|
| 137707 | 5N2828D000100 | USA (BLM) Site 3 | 77.27 | Federal Land |
| <u>124632</u> | <u>5N28C00001400</u> | <u>CLEAVER LAND, LLC</u> | 214 | Subject Property |
| <u>124632</u> | <u>5N28C00001400</u> | <u>CLEAVER LAND, LLC</u> | 106.34 | Subject Property |

Many of the parcels identified as Agricultural land are west of Powerline Road with better growth characteristics so have not been included for consideration. Most are captured within the study area having just a small portion of their acreage included. Two of the Agricultural parcels (Broken Spur) are situated in a location that make access difficult for industrial development.

Site 1 is Federal land under the management of the Confederated Tribes of the Umatilla Indian Reservation and is identified for future tribal uses with a Goal 11 exception in place. As Federal land it is not subject to Oregon’s statewide planning program and is not available to the City to direct economic opportunity. While the City of Umatilla would encourage economic opportunity to occur on this property it is unavailable for current inclusion in any inventory.

The McNary Dam and its associated Federal land holdings make up a large expanse of parks and natural areas. These areas would not be available for economic development opportunities. Site 2 lands are in Federal ownership, most under the purview of the Bureau of Indian Affairs. These properties are also protected in a partnership between the Confederated Tribes of the Umatilla Indian Reservation and the Bonneville Power Administration managed as the Wanaket Wildlife Mitigation Area (see attachment). There are also significant wetlands in this area, a portion identified within the Goal 5 inventory of Umatilla County.

There are two parcels in Federal ownership, managed by the Bureau of Land Management, identified as Site 3. Previous use of a portion of this land was a landfill operated many years ago and subsequently closed. Total acreage of the two parcels equals approximately 154 acres, driving its inclusion as an alternative Site.

See Exhibit E – supplemental findings addressing the City’s site selection analysis addressing OAR 660-02406605.

Conclusion: As addressed in this report three alternative locations have been determined and evaluated. The three alternative areas are 1) land to the east of the Port of Umatilla development and north of Highway 730 along the banks of the Columbia River (site 1), 2) land east and south of the Port of Umatilla and both north and south of Highway 730 (site 2), and 3) land to the south of the City of Umatilla between Highway 395 and Interstate 82 (site 3). In addition, as outline in the Exhibit E no other properties we identified as suitable alternative locations.

(2) A city that initiated the evaluation or amendment of its UGB prior to January 1, 2016, may choose to identify a preliminary study area applying the standard in this section rather than section (1). For such cities, the preliminary study area shall consist of:

- (a) All land adjacent to the acknowledged UGB, including all land in the vicinity of the UGB that has a reasonable potential to satisfy the identified need deficiency, and
- (b) All land in the city’s acknowledged urban reserve established under OAR chapter 660, division 21, if applicable.

Applicants Response: It is after January 1, 2016, making this provision not available.

Conclusion: It is after January 1, 2016, making this provision not available.

(3) When the primary purpose for expansion of the UGB is to accommodate a particular industrial use that requires specific site characteristics, or to accommodate a public facility that requires specific site characteristics, and the site characteristics may be found in only a small number of locations, the preliminary study area may be limited to those locations within the distance described in section (1) or (2), whichever is appropriate, that have or could be improved to provide the required site characteristics. For purposes of this section:

(a) The definition of “site characteristics” in OAR 660-009-0005(11) applies for purposes of identifying a particular industrial use.

(b) A “public facility” may include a facility necessary for public sewer, water, storm water, transportation, parks, schools, or fire protection. Site characteristics may include but are not limited to size, topography and proximity.

Applicants Response: This application is specifically designed to identify opportunities for large lot industrial development. While no specific industrial or public facility is identified, the Economic Opportunities Analysis calls out several industry clusters with this application focusing on data centers, light industrial manufacturing, and warehousing opportunities that require 50-100 acres or more than 100 acres. Based on this requirement, at least one of the alternative sites falls out of consideration as it does not have enough land to meet the total identified need – the site south of the City of Umatilla and west of Highway 395 (site 3).

Conclusion: Site 3 which is land to the south of the City of Umatilla between Highway 395 and Interstate 82, does not have enough acreage to meet the needed lot sizes as identified in the EOA, Site 3 is not considered a viable option.

(4) The city may exclude land from the preliminary study area if it determines that:

(a) Based on the standards in section (7) of this rule, it is impracticable to provide necessary public facilities or services to the land;

(b) The land is subject to significant development hazards, due to a risk of:

(A) Landslides: The land consists of a landslide deposit or scarp flank that is described and mapped on the Statewide Landslide Information Database for Oregon (SLIDO) Release 3.2 Geodatabase published by the Oregon Department of Geology and Mineral Industries (DOGAMI) December 2014, provided that the deposit or scarp flank in the data source is mapped at a scale of 1:40,000 or finer. If the owner of a lot or parcel provides the city with a site-specific analysis by a certified engineering geologist demonstrating that development of the property would not be subject to significant landslide risk, the city may not exclude the lot or parcel under this paragraph;

(B) Flooding, including inundation during storm surges: the land is within the Special Flood Hazard Area (SFHA) identified on the applicable Flood Insurance Rate Map (FIRM);

(C) Tsunamis: the land is within a tsunami inundation zone established pursuant to ORS 455.446;

(c) The land consists of a significant scenic, natural, cultural or recreational resource described in this subsection:

(A) Land that is designated in an acknowledged comprehensive plan prior to initiation of the UGB amendment, or that is mapped on a published state or federal inventory at a scale sufficient to determine its location for purposes of this rule, as:

- (i) Critical or essential habitat for a species listed by a state or federal agency as threatened or endangered;
- (ii) Core habitat for Greater Sage Grouse; or
- (iii) Big game migration corridors or winter range, except where located on lands designated as urban reserves or exception areas;
- (B) Federal Wild and Scenic Rivers and State Scenic Waterways, including Related Adjacent Lands described by ORS 390.805, as mapped by the applicable state or federal agency responsible for the scenic program;
- (C) Designated Natural Areas on the Oregon State Register of Natural Heritage Resources;
- (D) Wellhead protection areas described under OAR 660-023-0140 and delineated on a local comprehensive plan;
- (E) Aquatic areas subject to Statewide Planning Goal 16 that are in a Natural or Conservation management unit designated in an acknowledged comprehensive plan;
- (F) Lands subject to acknowledged comprehensive plan or land use regulations that implement Statewide Planning Goal 17, Coastal Shoreland, Use Requirement 1;
- (G) Lands subject to acknowledged comprehensive plan or land use regulations that implement Statewide Planning Goal 18, Implementation Requirement 2;
- (d) The land is owned by the federal government and managed primarily for rural uses.

Applicants Response: The alternative location (Site 2) east of the City of Umatilla lying both north and south of Highway 730 has significant wetlands with a portion specifically called out and protected within the Umatilla County Comprehensive Plan. Protection of wetlands and any required mitigation severely limit this site for development of large lot industrial activity, a primary objective of this application. Additionally, much of this area is also managed jointly between the Confederated Tribes of the Umatilla Indian Reservation and the Bonneville Power Administration as the Wanaket Wildlife Mitigation Area negatively impacting its availability for economic opportunity development.

The area east of the Port of Umatilla along the banks of the Columbia River (Site 1) does have an adopted Goal 11 exception which could be seen as making this an ideal location for large lot expansion. Current ownership is the primary factor in removing it from consideration as it is currently under Federal ownership and managed by the Confederated Tribes of the Umatilla Indian Reservation, is not subject to local land use authority or the statewide planning program, and is not available for development generally.

See Exhibit E – supplemental findings addressing the City’s site selection analysis addressing OAR 660-02406605.

Conclusion: Due to the wetlands that are inventoried on the National Wetland Inventory as well as in the Umatilla County’s Comprehensive plan found on Site 2, this alternative location becomes impracticable and not viable. Site 3 is currently owned and managed by the Confederated Tribes of the Umatilla Indian Reservation. While the City supports development on the CTUIR property, it is not subject to local land use authority or the state-wide planning goals. The City would have no authority to ensure the land was maintained or developed to meet the City’s need for large lot industrial sites.

(5) After excluding land from the preliminary study area under section (4), the city must adjust the area, if necessary, so that it includes an amount of land that is at least twice the amount of land

needed for the deficiency determined under OAR 660-024-0050(4) or, if applicable, twice the particular land need described in section (3). Such adjustment shall be made by expanding the distance specified under the applicable section (1) or (2) and applying section (4) to the expanded area.

Applicants Response: The table above identifies significant lands that have been considered. Much of the agricultural land has been excluded to not impact the local agricultural economy. The subject property (the approximate 150-acre urban growth boundary expansion), when combined with the other property that is part of the change in zoning request (approximately 300 acres), does accommodate the identified need as stated in the Economic Opportunities Analysis. The need is identified as two parcels in the range of 50 to 99.9 acres and a third parcel at over 100 acres. Given regional development trends that need could easily consume up to if not more than the 450 identified acres.

Conclusion: Given regional development trends that need could easily consume up to if not more than the 450 identified acres.

(6) For purposes of evaluating the priority of land under OAR 660-024-0067, the “study area” shall consist of all land that remains in the preliminary study area described in section (1), (2) or (3) of this rule after adjustments to the area based on sections (4) and (5), provided that when a purpose of the UGB expansion is to accommodate a public park need, the city must also consider whether land excluded under subsection (4)(a) through (c) of this rule can reasonably accommodate the park use.

Applicants Response: Parks are not a part of this application.

Conclusion: Parks are not a part of this application.

(7) For purposes of subsection (4)(a), the city may consider it impracticable to provide necessary public facilities or services to the following lands:

(a) Contiguous areas of at least five acres where 75 percent or more of the land has a slope of 25 percent or greater, provided that contiguous areas 20 acres or more that are less than 25 percent slope may not be excluded under this subsection. Slope shall be measured as the increase in elevation divided by the horizontal distance at maximum ten-foot contour intervals;

(b) Land that is isolated from existing service networks by physical, topographic, or other impediments to service provision such that it is impracticable to provide necessary facilities or services to the land within the planning period. The city’s determination shall be based on an evaluation of:

(A) The likely amount of development that could occur on the land within the planning period;

(B) The likely cost of facilities and services; and,

(C) Any substantial evidence collected by or presented to the city regarding how similarly situated land in the region has, or has not, developed over time.

(c) As used in this section, “impediments to service provision” may include but are not limited to:

(A) Major rivers or other water bodies that would require new bridge crossings to serve planned urban development;

(B) Topographic features such as canyons or ridges with slopes exceeding 40 percent and vertical relief of greater than 80 feet;

(C) Freeways, rail lines, or other restricted access corridors that would require new grade separated

crossings to serve planned urban development;

(D) Significant scenic, natural, cultural or recreational resources on an acknowledged plan inventory and subject to protection measures under the plan or implementing regulations, or on a published state or federal inventory, that would prohibit or substantially impede the placement or construction of necessary public facilities and services.

Applicants Response: The City of Umatilla had J-U-B Engineers complete an Umatilla Industrial Area Utility Technical Memorandum (dated March 2020) which states that the subject property, including the area that would be subject to the change in zoning, can be served with water, wastewater and industrial wastewater. While there is slope on the subject property it is limited to the eastern edge, sloping down to Interstate 82. Most of the property, particularly the frontage along Powerline Road, is reasonably flat.

Conclusion: The City of Umatilla had J-U-B Engineers complete an Umatilla Industrial Area Utility Technical Memorandum (March 2020) which states that the subject property, including the area that would be subject to the change in zoning, can be served with water, wastewater and industrial wastewater. According to the UTM, the subject property has been deemed viable to be served with water, wastewater and industrial wastewater.

(8) Land may not be excluded from the preliminary study area based on a finding of impracticability that is primarily a result of existing development patterns. However, a city may forecast development capacity for such land as provided in OAR 660-024-0067(1)(d).

Applicants Response: Current development patterns were not a consideration in the application process. The three alternative Sites are currently bare. Development east of Umatilla, which includes alternative Sites 1 and 2, consists of significant land in Federal ownership, current economic development within the Port of Umatilla, various agricultural activities, and land maintained for habitat values. The alternative Site 3 south of Umatilla was deemed too small to meet the need, is in Federal ownership, and is configured long and narrow, which could be a hinderance to larger lot development opportunities.

Conclusion: Development patterns were not applicable to the three alternative sites, as they are currently bare.

(9) Notwithstanding OAR 660-024-0050(4) and section (1) of this rule, except during periodic review or other legislative review of the UGB, the city may approve an application under ORS 197.610 to 197.625 for a UGB amendment to add an amount of land less than necessary to satisfy the land need deficiency determined under OAR 660-024-0050(4), provided the amendment complies with all other applicable requirements.

Applicants Response: This application is not a part of the City of Umatilla's periodic review. It is submitted to meet a specific need of large lot industrial land as outlined in the Economic Opportunities Analysis that is included as part of the application. The amount of land included in the urban growth boundary expansion (150 acres), when coupled with the land in the associated change of zoning request (300 acres), meets the stated need for large lot industrial land within the Economic Opportunities Analysis

Conclusion: Neither periodic review or other legislative review of the UGB is being conducted. Filling the need of large lot industrial land highlighted by the Economic Opportunities Analysis is the purpose of this application. As addressed above the subject property is large enough to satisfy the land need deficiency as determined under OAR 660-024-0050(4).

Oregon Administrative Rule 660 Division 24 Section 0067 Evaluation of Land in the Study Area for Inclusion in the UGB continues this analysis.

See Exhibit E – See supplemental findings providing additional analysis for City’s site evaluation analysis addressing OAR 660-02406607.

- (1) A city considering a UGB amendment must decide which land to add to the UGB by evaluating all land in the study area determined under OAR 660-024-0065, as follows
- (a) Beginning with the highest priority category of land described in section (2), the city must apply section (5) to determine which land in that priority category is suitable to satisfy the need deficiency determined under OAR 660-024-0050 and select for inclusion in the UGB as much of the land as necessary to satisfy the need.
- (b) If the amount of suitable land in the first priority category is not sufficient to satisfy all the identified need deficiency, the city must apply section (5) to determine which land in the next priority is suitable and select for inclusion in the UGB as much of the suitable land in that priority as necessary to satisfy the need. The city must proceed in this manner until all the land need is satisfied, except as provided in OAR 660-024-0065(9).
- (c) If the amount of suitable land in a particular priority category in section (2) exceeds the amount necessary to satisfy the need deficiency, the city must choose which land in that priority to include in the UGB by applying the criteria in section (7) of this rule.
- (d) In evaluating the sufficiency of land to satisfy a need under this section, the city may use the factors identified in sections (5) and (6) of this rule to reduce the forecast development capacity of the land to meet the need.
- (e) Land that is determined to not be suitable under section (5) of this rule to satisfy the need deficiency determined under OAR 660-024-0050 is not required to be selected for inclusion in the UGB unless its inclusion is necessary to serve other higher priority lands.

Applicants Response: This application is focused on an urban growth boundary amendment for large lot industrial development. This need was identified in the attached Economic Opportunities Analysis completed for the City of Umatilla in October 2019. The requirements of OAR 660-024-0065 are addressed above. The alternative sites identified in the section above where shown to have limitations removing them from consideration. The subject site meets the identified need for two sites between 50 and 99.9 acres and a two sites over 100 acres. When regional patterns are considered for development patterns that need could easily be 450 acres.

Conclusion: The lack of large lot industrial parcels as identified in the Economic Opportunities Analysis can be met by the submitted applications. The requirements of OAR 660-024-0065 are addressed above. The subject site meets the identified need for 300 – 399.98 acres of land suitable for large lot industrial development as outlined in the Economic Opportunities Analysis. The subject property for inclusion and rezoning totals 450+/- acres.

(2) Priority of Land for inclusion in a UGB:

(a) First Priority is urban reserve, exception land, and nonresource land. Lands in the study area that meet the description in paragraphs (A) through (C) of this subsection are of equal (first) priority:

(A) Land designated as an urban reserve under OAR chapter 660, division 21, in an acknowledged comprehensive plan;

(B) Land that is subject to an acknowledged exception under ORS 197.732; and

(C) Land that is nonresource land.

Applicants Response: The City of Umatilla does not have any urban reserves; no lands with an acknowledged exception are available (the parcel with the Goal 11 exception is owned or managed by the Confederated Tribes of the Umatilla Indian Reservation, is not subject to local land use authority, and is not available for development to meet current needs); and no other non-resource land has been identified as being available or of sufficient size to meet the identified need.

See supplemental findings for additional site evaluation analysis.

Conclusion: The supplemental finding show two alternative locations would be considered first priority land for inclusion. However, those properties were not determined to be suitable to meet the City's identified need for large lot industrial sites.

(b) Second Priority is marginal land: land within the study area that is designated as marginal land under ORS 197.247 (1991 Edition) in the acknowledged comprehensive plan.

Applicants Response: There are no designated marginal lands within Umatilla County.

Conclusion: There are no designated marginal lands within Umatilla County.

(c) Third Priority is forest or farm land that is not predominantly high-value farm land: land within the study area that is designated for forest or agriculture uses in the acknowledged comprehensive plan and that is not predominantly high-value farmland as defined in ORS 195.300, or that does not consist predominantly of prime or unique soils, as determined by the United States Department of Agriculture Natural Resources Conservation Service (USDA NRCS). In selecting which lands to include to satisfy the need, the city must use the agricultural land capability classification system or the cubic foot site class system, as appropriate for the acknowledged comprehensive plan designation, to select lower capability or cubic foot site class lands first.

Applicants Response: There are no Goal 4 or Forest Lands adjacent to the City of Umatilla. Already excluded are areas with wetlands and an area not of sufficient size to accommodate the need. The subject area is comprised of Class VIIe Soils if not irrigated. Specifically, the soils are Burbank loamy find sand with 0 to 5 percent slopes for the area to the west and Quincy loamy find sand with 5 to 25 percent slopes for the area to the east. The lands are not considered prime or unique.

Conclusion: The supplemental findings clarify the soils for all other properties surrounding the

UGB considered as part of the site evaluation analysis are considered high-value farmland as defined by ORS 195.300. Therefore, no properties were identified as Third Priority land for inclusion.

(d) Fourth Priority is agricultural land that is predominantly high-value farmland: land within the study area that is designated as agricultural land in an acknowledged comprehensive plan and is predominantly high-value farmland as defined in ORS 195.300. A city may not select land that is predominantly made up of prime or unique farm soils, as defined by the USDA NRCS, unless there is an insufficient amount of other land to satisfy its land need. In selecting which lands to include to satisfy the need, the city must use the agricultural land capability classification system to select lower capability lands first.

Applicants Response: The land is not identified as high-value farmland, nor is it prime or unique. The approximate 150 acres identified for inclusion within the urban growth boundary is currently farmed with only about half under pivot irrigation. The balance is scrub land, unavailable based on the shape of the ownership and layout options for pivot irrigation. The most easterly portion of the property slopes down to Interstate 82.

Conclusion: As addressed in detail in the supplemental findings there are three sites identified as suitable alternative locations. The properties identified as East 395 property are located on class 1 soils and contained mapped wetlands as identified on both the National Wetlands Inventory and State Wetlands Inventory. Given these properties are the only site identified within the study area with class 1 soils the East 395 properties were not considered a suitable alternative location. The Topaz Land properties and the Cleaver Land properties are both considered high-value farmland given they have existing water rights on designated agricultural land. The supplemental findings conclude that the Cleaver Land properties would impact less high-value farmland than inclusion of the Topaz Lan properties as any expansion of the UGB would impact lands in irrigation crop production. A significant portion of the irrigated farm land that would be removed from production on the Cleaver land properties are already located within the UGB and therefore, no considered high-value farmland.

(3) Notwithstanding section (2)(c) or (d) of this rule, land that would otherwise be excluded from a UGB may be included if:

(a) The land contains a small amount of third or fourth priority land that is not important to the commercial agricultural enterprise in the area and the land must be included in the UGB to connect a nearby and significantly larger area of land of higher priority for inclusion within the UGB; or

(b) The land contains a small amount of third or fourth priority land that is not predominantly high-value farmland or predominantly made up of prime or unique farm soils and the land is completely surrounded by land of higher priority for inclusion into the UGB.

Applicants Response: This action does not seek to connect an area nor is it surrounded by land of higher priority. This action seeks to add approximately 150 acres to the urban growth boundary of which about half is under circle pivot irrigation, the balance scrub land not available for irrigation based on the shape and layout of the ownership. None of the land is prime or unique.

Conclusion: The above standards do not apply to the subject property.

(4) For purposes of categorizing and evaluating land pursuant to subsections (2)(c) and (d) and section (3) of this rule,

(a) Areas of land not larger than 100 acres may be grouped together and studied as a single unit of land;

(b) Areas of land larger than 100 acres that are similarly situated and have similar soils may be grouped together provided soils of lower agricultural or forest capability may not be grouped with soils of higher capability in a manner inconsistent with the intent of section (2) of this rule, which requires that higher capability resource lands shall be the last priority for inclusion in a UGB;

(c) Notwithstanding subsection (4)(a), if a city initiated the evaluation or amendment of its UGB prior to January 1, 2016, and if the analysis involves more than one lot or parcel or area within a particular priority category for which circumstances are reasonably similar, these lots, parcels and areas may be considered and evaluated as a single group;

(d) When determining whether the land is predominantly high-value farmland, or predominantly prime or unique, “predominantly” means more than 50 percent.

Applicants Response: The land is not identified as high-value farmland, nor is it prime or unique. This action seeks to add approximately 150 acres to the urban growth boundary of which about half is under circle pivot irrigation, the balance scrub land not available for irrigation based on the shape and layout of the ownership.

Conclusion: As addressed in the supplemental findings three areas were identified as high-value farmland. The subject property was determined to be the most suitable location and would impact the least amount of high-value farmland.

(5) With respect to section (1), a city must assume that vacant or partially vacant land in a particular priority category is “suitable” to satisfy a need deficiency identified in OAR 660-024-0050(4) unless it demonstrates that the land cannot satisfy the specified need based on one or more of the conditions described in subsections (a) through (g) of this section:

(a) Existing parcelization, lot sizes or development patterns of rural residential land make that land unsuitable for an identified employment need; as follows:

(A) Parcelization: the land consists primarily of parcels 2-acres or less in size, or

(B) Existing development patterns: the land cannot be reasonably redeveloped or infilled within the planning period due to the location of existing structures and infrastructure.

(b) The land would qualify for exclusion from the preliminary study area under the factors in OAR 660-024-0065(4) but the city declined to exclude it pending more detailed analysis.

(c) The land is, or will be upon inclusion in the UGB, subject to natural resources protections under Statewide Planning Goal 5 such that that no development capacity should be forecast on that land to meet the land need deficiency.

(d) With respect to needed industrial uses only, the land is over 10 percent slope, or is an existing lot or parcel that is smaller than 5 acres in size, or both. Slope shall be measured as the increase in elevation divided by the horizontal distance at maximum ten-foot contour intervals.

(e) With respect to a particular industrial use or particular public facility use described in OAR 660-024-0065(3), the land does not have, and cannot be improved to provide, one or more of the required specific site characteristics.

(f) The land is subject to a conservation easement described in ORS 271.715 that prohibits urban development.

(g) The land is committed to a use described in this subsection and the use is unlikely to be discontinued during the planning period:

(A) Public park, church, school, or cemetery, or

(B) Land within the boundary of an airport designated for airport uses, but not including land designated or zoned for residential, commercial or industrial uses in an acknowledged comprehensive plan.

Applicants Response: None of the alternative sites have been parcelized. The alternative site east of the City of Umatilla lying both north and south of Highway 730 (site 2) has significant wetlands, some identified within the Umatilla County Comprehensive Plan, that would be subject to development restrictions limiting opportunities for large lot industrial development. The alternative site south of the City of Umatilla and west of Highway 395 (site 3) is about 160 acres, long and narrow, which could limit large lot development and not of sufficient size to fulfill the need as identified within the Economic Opportunities Analysis. The subject site is of a size and shape to meet the needs as outlined in the Economic Opportunities Analysis.

Conclusion: Due to wetlands on site 2 and the lot sizes and shapes of site 3, the subject site is the only one that is a size and shape to meet the needs as outlined in the Economic Opportunities Analysis.

(6) For vacant or partially vacant lands added to the UGB to provide for residential uses:

(a) Existing lots or parcels one acre or less may be assumed to have a development capacity of one dwelling unit per lot or parcel. Existing lots or parcels greater than one acre but less than two acres shall be assumed to have an aggregate development capacity of two dwelling units per acre.

(b) In any subsequent review of a UGB pursuant to this division, the city may use a development assumption for land described in subsection (a) of this section for a period of up to 14 years from the date the lands were added to the UGB.

Applicants Response: This is not applicable as the intent is to create opportunities for large lot industrial uses.

Conclusion: This is not applicable as the intent is to create opportunities for the identified need for large lot industrial uses.

(7) Pursuant to subsection (1)(c), if the amount of suitable land in a particular priority category under section (2) exceeds the amount necessary to satisfy the need deficiency, the city must choose which land in that priority to include in the UGB by first applying the boundary location factors of Goal 14 and then applying applicable criteria in the acknowledged comprehensive plan and land use regulations acknowledged prior to initiation of the UGB evaluation or amendment. The city may not apply local comprehensive plan criteria that contradict the requirements of the boundary location factors of Goal 14. The boundary location factors are not independent criteria; when the factors are applied to compare alternative boundary locations and to determine the UGB location the city must show that it considered and balanced all the factors. The criteria in this section may not be used to select lands designated for agriculture or forest use that have higher land capability or cubic foot site class, as applicable, ahead of lands that have lower capability or cubic foot site class.

Applicants Response: No forest lands are being considered. The land classification of the subject area is Class VIIe, not high-value, prime or unique. The applicant would assert that the subject site balances the need for industrial land against other land needs.

Conclusion: No forest lands are being considered. As address in the supplemental finding no properties were identified that do not include high-value farmland. The subject site balances the need for industrial land against other land needs.

(8) The city must apply the boundary location factors of Goal 14 in coordination with service providers and state agencies, including the Oregon Department of Transportation (ODOT) with respect to Factor 2 regarding impacts on the state transportation system, and the Oregon Department of Fish and Wildlife (ODFW) and the Department of State Lands (DSL) with respect to Factor 3 regarding environmental consequences. “Coordination” includes timely notice to agencies and service providers and consideration of any recommended evaluation methodologies.

Applicants Response: The Oregon Department of Transportation was contacted early in the application process. The applicant anticipates that both the Oregon Department of Fish and Wildlife and Department of State Lands will be provided notice of the required public hearings to consider this application. The Department of Land Conservation and Development has been involved through pre-application contact and meetings.

Conclusion: The City of Umatilla noticed the above agencies on August 4, 2020.

(9) In applying Goal 14 Boundary Location Factor 2 to evaluate alternative locations under section (7), the city must compare relative costs, advantages and disadvantages of alternative UGB expansion areas with respect to the provision of public facilities and services needed to urbanize alternative boundary locations. For purposes of this section, the term “public facilities and services” means water, sanitary sewer, storm water management, and transportation facilities. The evaluation and comparison under Boundary Location Factor 2 must consider:

- (a) The impacts to existing water, sanitary sewer, storm water and transportation facilities that serve nearby areas already inside the UGB;
- (b) The capacity of existing public facilities and services to serve areas already inside the UGB as well as areas proposed for addition to the UGB; and
- (c) The need for new transportation facilities, such as highways and other roadways, interchanges, arterials and collectors, additional travel lanes, other major improvements on existing roadways and, for urban areas of 25,000 or more, the provision of public transit service.

Applicants Response: An Umatilla Industrial Area Utility Technical Memorandum was completed for the subject area concluding that public services can be reasonably provided. That memorandum evaluated water, wastewater, industrial process water, and the option of irrigation water. Also evaluated was how a connection to the Umatilla Army Depot reuse areas could create efficiencies and synergies. No other area was evaluated as they were eliminated from consideration for the reasons discussed above.

Conclusion: The Utility Technical Memorandum states that water, wastewater, industrial wastewater can be reasonably provided to the subject property. No other area was evaluated as they were eliminated from consideration for the reasons discussed above.

(10) The adopted findings for UGB amendment must describe or map all of the alternative areas evaluated in the boundary location alternatives analysis.

Applicants Response: Please see the included Study Area map.

Conclusion: Please see the included Study Area map.

Oregon Administrative Rule Chapter 660 Division 12 Section 0060 governs Plan and Land Use Regulation Amendments.

(1) If an amendment to a functional plan, an acknowledged comprehensive plan, or a land use regulation (including a zoning map) would significantly affect an existing or planned transportation facility, then the local government must put in place measures as provided in section (2) of this rule, unless the amendment is allowed under section (3), (9) or (10) of this rule. A plan or land use regulation amendment significantly affects a transportation facility if it would:

(a) Change the functional classification of an existing or planned transportation facility (exclusive of correction of map errors in an adopted plan);

(b) Change standards implementing a functional classification system; or

(c) Result in any of the effects listed in paragraphs (A) through (C) of this subsection based on projected conditions measured at the end of the planning period identified in the adopted TSP. As part of evaluating projected conditions, the amount of traffic projected to be generated within the area of the amendment may be reduced if the amendment includes an enforceable, ongoing requirement that would demonstrably limit traffic generation, including, but not limited to, transportation demand management. This reduction may diminish or completely eliminate the significant effect of the amendment.

(A) Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility;

(B) Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or

(C) Degrade the performance of an existing or planned transportation facility that is otherwise projected to not meet the performance standards identified in the TSP or comprehensive plan.

Applicants Response: As part of the application process the City of Umatilla accomplished a Traffic Impact Analysis (TIA), completed by J-U-B Engineers and dated May 2020. The TIA comes to several conclusions, summarized on page 17 of the Analysis, concerning the function of Powerline Road as well as its connection to both Interstate 82 and Highway 730. The effect of the urban growth boundary expansion and annexation, when coupled with the change in zoning, results in a net reduction in daily traffic including the pm peak hour (this is further discussed on page 7 of the TIA). The analysis does conclude there will be impacts to intersections at the Interstate 82 Interchange and the intersection with Highway 730. For this particular criterion the applicant would assert that the TIA provides evidence that Powerline Road along the frontage of the subject property does not require a change in functional classification or the standards to implement the functional classification, and in fact results in a lower pm peak hour by nearly 800

trips in 2040.

Comment has been received from the Oregon Department of Transportation dated August 21, 2020, and signed by Marilyn Holt, District 12 Manager (see attached letter). The letter provides the following guidance to the City of Umatilla, “Page 17 of the TIA identifies the intersection of Powerline Road/US 730 will need a higher level of traffic control such as a traffic signal or roundabout. Also, both a southbound right-turn lane at the southbound Interstate-82 ramps and a southbound left-turn will be needed at the Interstate-82 northbound ramp. Accordingly to reflect long-term changes with appropriate improvements, balancing access and circulation management require context sensitive designs to respond to growth. As this area urbanizes, frontage improvement, such as transit facilities, curb, sidewalk, crosswalk ramps(s), bikeways and street standards should be constructed as necessary to provide travel choices and to be consistent with the City’s Transportation System Plan (TSP) and ADA standards. ODOT recommends these elements should be addressed with emphasis on development contributing to implement the improvements that may be necessary to provide safe and acceptable Levels of Service in order to meet City and ODOT standards.” The applicant addresses these items in other locations within this narrative stating that City of Umatilla development standards, including requirements within the Transportation System Plan, would be applicable at the time of development, requiring many of these development components to be installed. There is also discussion within this narrative that connections to the recently adopted trails system within the City of Umatilla is possible with this development as it occurs over time. Residential development that has been occurring north of this location within the city limits has required developers to install curb, gutter and sidewalks along with widening of Powerline Road. It is anticipated that the City would require similar installations as part of any industrial development on the subject property.

Conclusion: The effect of the urban growth boundary expansion and annexation, when coupled with the change in zoning, results in a net reduction in daily traffic including the pm peak hour for the subject property.

(2) If a local government determines that there would be a significant effect, then the local government must ensure that allowed land uses are consistent with the identified function, capacity, and performance standards of the facility measured at the end of the planning period identified in the adopted TSP through one or a combination of the remedies listed in (a) through (e) below, unless the amendment meets the balancing test in subsection (2)(e) of this section or qualifies for partial mitigation in section (11) of this rule. A local government using subsection (2)(e), section (3), section (10) or section (11) to approve an amendment recognizes that additional motor vehicle traffic congestion may result and that other facility providers would not be expected to provide additional capacity for motor vehicles in response to this congestion.

(a) Adopting measures that demonstrate allowed land uses are consistent with the planned function, capacity, and performance standards of the transportation facility.

(b) Amending the TSP or comprehensive plan to provide transportation facilities, improvements or services adequate to support the proposed land uses consistent with the requirements of this division; such amendments shall include a funding plan or mechanism consistent with section (4) or include an amendment to the transportation finance plan so that the facility, improvement, or service will be provided by the end of the planning period.

(c) Amending the TSP to modify the planned function, capacity or performance standards

of the transportation facility.

(d) Providing other measures as a condition of development or through a development agreement or similar funding method, including, but not limited to, transportation system management measures or minor transportation improvements. Local governments shall, as part of the amendment, specify when measures or improvements provided pursuant to this subsection will be provided.

(e) Providing improvements that would benefit modes other than the significantly affected mode, improvements to facilities other than the significantly affected facility, or improvements at other locations, if:

(A) The provider of the significantly affected facility provides a written statement that the system-wide benefits are sufficient to balance the significant effect, even though the improvements would not result in consistency for all performance standards;

(B) The providers of facilities being improved at other locations provide written statements of approval; and

(C) The local jurisdictions where facilities are being improved provide written statements of approval.

Applicants Response: The TIA identifies that the function along Powerline Road could be improved based on this action. The intersections with both Interstate 82 and Highway 730 do not fare as well and will need improvements particularly when combined with the assumed background growth along Powerline Road. Specifically, the TIA calls for signalization or a round-about at the Powerline Road and Highway 730 intersection stating that, “The traffic signal would likely be required at about 10 years of background growth and 50% of the site generated trips if the low-cost improvements described above were implemented.”

The TIA also call for work at the Interstate 82 Interchange as follows, “A southbound right turn at the southbound I-82 ramps will be needed at approximately 80% of the background growth and 80% of the industrial development. A southbound left turn will be needed at the I-82 northbound ramps at approximately 33% of the background growth and 33% of the industrial development.”

The applicant would assert that the TIA provides evidence that the proposed urban growth boundary expansion and annexation along with the change in zoning would not significantly impact the identified function, capacity, and performance of Powerline Road. There will be impacts to the intersections with both Interstate 82 and Highway 730 at a future point in time based on both background growth and development of the proposed industrial area. The applicant expects to work with the City and other transportation providers to assure that necessary projects are identified for inclusion in the City and County Transportation System Plans. Funding for those projects could be secured through system development charges on industrial projects on the subject site.

Conclusion: The TIA provides evidence that the proposed urban growth boundary expansion and annexation along with the change in zoning would not significantly impact the identified function, capacity, and performance of Powerline Road. There will be impacts to the intersections with both Interstate 82 and Highway 730 at a future point in time based on both background growth and development of the proposed industrial area. Funding for those projects could be secured through system development charges on industrial projects on the subject site.

(3) Notwithstanding sections (1) and (2) of this rule, a local government may approve an amendment that would significantly affect an existing transportation facility without assuring that the allowed land uses are consistent with the function, capacity and performance standards of the facility where:

(a) In the absence of the amendment, planned transportation facilities, improvements and services as set forth in section (4) of this rule would not be adequate to achieve consistency with the identified function, capacity or performance standard for that facility by the end of the planning period identified in the adopted TSP;

(b) Development resulting from the amendment will, at a minimum, mitigate the impacts of the amendment in a manner that avoids further degradation to the performance of the facility by the time of the development through one or a combination of transportation improvements or measures;

(c) The amendment does not involve property located in an interchange area as defined in paragraph (4)(d)(C); and

(d) For affected state highways, ODOT provides a written statement that the proposed funding and timing for the identified mitigation improvements or measures are, at a minimum, sufficient to avoid further degradation to the performance of the affected state highway. However, if a local government provides the appropriate ODOT regional office with written notice of a proposed amendment in a manner that provides ODOT reasonable opportunity to submit a written statement into the record of the local government proceeding, and ODOT does not provide a written statement, then the local government may proceed with applying subsections (a) through (c) of this section.

Applicants Response: The portion of Powerline Road that fronts the subject property is a paved county road, is not a state highway, nor is it within an interchange area or within an area with an adopted Interchange Area Management Plan. Based on the TIA the applicant would assert that Powerline Road is not significantly impacted by the urban growth boundary expansion and annexation, with the change in zoning providing a lower pm peak hour improving the future function of Powerline Road. The applicant does acknowledge the future impacts to the intersections of Powerline Road with both Interstate-82 and Highway 730. See the included comment letter from the Oregon Department of Transportation, dated August 21, 2020, and signed by Marilyn Holt, District 12 Manager.

Conclusion: Powerline Road is not significantly impacted by the urban growth boundary expansion and annexation. Future development will have impacts to the intersections on powerline road, these will be addressed at time of development.

(4) Determinations under sections (1)–(3) of this rule shall be coordinated with affected transportation facility and service providers and other affected local governments.

(a) In determining whether an amendment has a significant effect on an existing or planned transportation facility under subsection (1)(c) of this rule, local governments shall rely on existing transportation facilities and services and on the planned transportation facilities, improvements and services set forth in subsections (b) and (c) below.

(b) Outside of interstate interchange areas, the following are considered planned facilities, improvements and services:

(A) Transportation facilities, improvements or services that are funded for

construction or implementation in the Statewide Transportation Improvement Program or a locally or regionally adopted transportation improvement program or capital improvement plan or program of a transportation service provider.

(B) Transportation facilities, improvements or services that are authorized in a local transportation system plan and for which a funding plan or mechanism is in place or approved. These include, but are not limited to, transportation facilities, improvements or services for which: transportation systems development charge revenues are being collected; a local improvement district or reimbursement district has been established or will be established prior to development; a development agreement has been adopted; or conditions of approval to fund the improvement have been adopted.

(C) Transportation facilities, improvements or services in a metropolitan planning organization (MPO) area that are part of the area's federally-approved, financially constrained regional transportation system plan.

(D) Improvements to state highways that are included as planned improvements in a regional or local transportation system plan or comprehensive plan when ODOT provides a written statement that the improvements are reasonably likely to be provided by the end of the planning period.

(E) Improvements to regional and local roads, streets or other transportation facilities or services that are included as planned improvements in a regional or local transportation system plan or comprehensive plan when the local government(s) or transportation service provider(s) responsible for the facility, improvement or service provides a written statement that the facility, improvement or service is reasonably likely to be provided by the end of the planning period.

(c) Within interstate interchange areas, the improvements included in (b)(A)–(C) are considered planned facilities, improvements and services, except where:

(A) ODOT provides a written statement that the proposed funding and timing of mitigation measures are sufficient to avoid a significant adverse impact on the Interstate Highway system, then local governments may also rely on the improvements identified in paragraphs (b)(D) and (E) of this section; or

(B) There is an adopted interchange area management plan, then local governments may also rely on the improvements identified in that plan and which are also identified in paragraphs (b)(D) and (E) of this section.

(d) As used in this section and section (3):

(A) Planned interchange means new interchanges and relocation of existing interchanges that are authorized in an adopted transportation system plan or comprehensive plan;

(B) Interstate highway means Interstates 5, 82, 84, 105, 205 and 405; and

(C) Interstate interchange area means:

(i) Property within one-quarter mile of the ramp terminal intersection of an existing or planned interchange on an Interstate Highway; or

(ii) The interchange area as defined in the Interchange Area Management Plan adopted as an amendment to the Oregon Highway Plan.

(e) For purposes of this section, a written statement provided pursuant to paragraphs (b)(D), (b)(E) or (c)(A) provided by ODOT, a local government or transportation facility provider, as appropriate, shall be conclusive in determining whether a transportation facility, improvement or service is a planned transportation facility, improvement or

service. In the absence of a written statement, a local government can only rely upon planned transportation facilities, improvements and services identified in paragraphs (b)(A)–(C) to determine whether there is a significant effect that requires application of the remedies in section (2).

Applicants Response: The subject area proposed for inclusion within the City of Umatilla urban growth boundary and city limits, and the larger project area proposed for a change in Zoning to Light Industrial, are located north approximately one-half mile of the Powerline Road interchange on Interstate 82. There is no adopted Interchange Area Management Plan and no corresponding interchange area that has been applied.

The portion of Powerline Road fronting the subject property is an Umatilla County paved road (City if annexation approved). Based on the Joint Management Agreement between Umatilla County and the City of Umatilla a portion of Powerline Road has been transferred from the County to the City. Both Umatilla County and City of Umatilla transportation standards are discussed more fully later in this narrative.

The applicant asserts that the TIA provides evidence that the impacts to Powerline Road are an improvement to the pm peak hour. The applicant also asserts that the proposed changes are at least one-half mile from the Interstate-82 Interchange.

Conclusion: There is no adopted Interchange Area Management Plan and no corresponding interchange area that has been applied. The proposed changes are at least one-half mile from the Interstate-82 Interchange. Upon approval of UGB expansion, the portion of powerline road adjacent to the subject property will be subject to City of Umatilla transportation standards.

(5) The presence of a transportation facility or improvement shall not be a basis for an exception to allow residential, commercial, institutional or industrial development on rural lands under this division or OAR 660-004-0022 and 660-004-0028.

Applicants Response: This is not an application to allow industrial development on rural lands, but an application package seeking an expansion of the City of Umatilla urban growth boundary and annexation along with an associated application requesting a change in Zoning to Light Industrial. While the Powerline Road Interchange on Interstate 82 is a beneficial transportation improvement, it is not the sole or primary reason for these applications.

Conclusion: This application package is to expand the City of Umatilla urban growth boundary to allow for more large lot industrial parcels. This application is not to allow industrial development on rural lands.

(6) In determining whether proposed land uses would affect or be consistent with planned transportation facilities as provided in sections (1) and (2), local governments shall give full credit for potential reduction in vehicle trips for uses located in mixed-use, pedestrian-friendly centers, and neighborhoods as provided in subsections (a)–(d) below;

(a) Absent adopted local standards or detailed information about the vehicle trip reduction benefits of mixed-use, pedestrian-friendly development, local governments shall assume that uses located within a mixed-use, pedestrian-friendly center, or

neighborhood, will generate 10% fewer daily and peak hour trips than are specified in available published estimates, such as those provided by the Institute of Transportation Engineers (ITE) Trip Generation Manual that do not specifically account for the effects of mixed-use, pedestrian-friendly development. The 10% reduction allowed for by this section shall be available only if uses which rely solely on auto trips, such as gas stations, car washes, storage facilities, and motels are prohibited;

(b) Local governments shall use detailed or local information about the trip reduction benefits of mixed-use, pedestrian-friendly development where such information is available and presented to the local government. Local governments may, based on such information, allow reductions greater than the 10% reduction required in subsection (a) above;

(c) Where a local government assumes or estimates lower vehicle trip generation as provided in subsection (a) or (b) above, it shall assure through conditions of approval, site plans, or approval standards that subsequent development approvals support the development of a mixed-use, pedestrian-friendly center or neighborhood and provide for on-site bike and pedestrian connectivity and access to transit as provided for in OAR 660-012-0045(3) and (4). The provision of on-site bike and pedestrian connectivity and access to transit may be accomplished through application of acknowledged ordinance provisions which comply with 660-012-0045(3) and (4) or through conditions of approval or findings adopted with the plan amendment that assure compliance with these rule requirements at the time of development approval; and

(d) The purpose of this section is to provide an incentive for the designation and implementation of pedestrian-friendly, mixed-use centers and neighborhoods by lowering the regulatory barriers to plan amendments which accomplish this type of development. The actual trip reduction benefits of mixed-use, pedestrian-friendly development will vary from case to case and may be somewhat higher or lower than presumed pursuant to subsection (a) above. The Commission concludes that this assumption is warranted given general information about the expected effects of mixed-use, pedestrian-friendly development and its intent to encourage changes to plans and development patterns. Nothing in this section is intended to affect the application of provisions in local plans or ordinances which provide for the calculation or assessment of systems development charges or in preparing conformity determinations required under the federal Clean Air Act.

Applicants Response: The proposed uses are industrial in nature – data centers, light manufacturing and warehousing – with traffic impacts addressed in the Traffic Impact Study for these activities. The growth of residential activity to the north of the subject property does include development of sidewalks and bicycle facilities along Powerline Road that could be connected to the proposed industrial area, creating a pedestrian and bicycle connection to the commercial and downtown area of the City of Umatilla. It is not known what the potential is for workers within the industrial area to either walk or bicycle to work, but that potential does exist and should be acknowledged. The proposed development can be connected to Powerline Road and the trail network that has been adopted by the City of Umatilla.

Conclusion: The proposed development can be connected to Powerline Road and the trail network that has been adopted by the City of Umatilla. Bike and pedestrian standards will be enforced at the time of development.

(7) Amendments to acknowledged comprehensive plans and land use regulations which meet all of the criteria listed in subsections (a)–(c) below shall include an amendment to the comprehensive plan, transportation system plan, the adoption of a local street plan, access management plan, future street plan or other binding local transportation plan to provide for on-site alignment of streets or accessways with existing and planned arterial, collector, and local streets surrounding the site as necessary to implement the requirements in OAR 660-012-0020(2)(b) and 660-012-0045(3):

(a) The plan or land use regulation amendment results in designation of two or more acres of land for commercial use;

(b) The local government has not adopted a TSP or local street plan which complies with OAR 660-012-0020(2)(b) or, in the Portland Metropolitan Area, has not complied with Metro's requirement for street connectivity as contained in Title 6, Section 3 of the Urban Growth Management Functional Plan; and

(c) The proposed amendment would significantly affect a transportation facility as provided in section (1).

Applicants Response: This request is proposed to result in land designated Light Industrial, the City of Umatilla has an adopted Transportation System Plan and the Traffic Impact Analysis determined that there is a reduction in pm peak hour traffic. The applicant asserts that this criterion would not be applicable to this action.

Conclusion: Due to the adopted TSP and provided TIA, this criterion is not applicable.

(8) A "mixed-use, pedestrian-friendly center or neighborhood" for the purposes of this rule, means:

(a) Any one of the following:

(A) An existing central business district or downtown;

(B) An area designated as a central city, regional center, town center or main street in the Portland Metro 2040 Regional Growth Concept;

(C) An area designated in an acknowledged comprehensive plan as a transit oriented development or a pedestrian district; or

(D) An area designated as a special transportation area as provided for in the Oregon Highway Plan.

(b) An area other than those listed in subsection (a) above which includes or is planned to include the following characteristics:

(A) A concentration of a variety of land uses in a well-defined area, including the following:

(i) Medium to high density residential development (12 or more units per acre);

(ii) Offices or office buildings;

(iii) Retail stores and services;

(iv) Restaurants; and

(v) Public open space or private open space which is available for public use, such as a park or plaza.

(B) Generally include civic or cultural uses;

(C) A core commercial area where multi-story buildings are permitted;

(D) Buildings and building entrances oriented to streets;

(E) Street connections and crossings that make the center safe and conveniently accessible from adjacent areas;

(F) A network of streets and, where appropriate, accessways and major driveways that make it attractive and highly convenient for people to walk between uses within the center or neighborhood, including streets and major driveways within the center with wide sidewalks and other features, including pedestrian-oriented street crossings, street trees, pedestrian-scale lighting and on-street parking;

(G) One or more transit stops (in urban areas with fixed route transit service); and

(H) Limit or do not allow low-intensity or land extensive uses, such as most industrial uses, automobile sales and services, and drive-through services.

Applicants Response: This proposal, if approved, will result in an industrial area Zoned Light Industrial. It is not proposed as a mixed-use area but could connect to the sidewalk or bicycle paths that are being incorporated along Powerline Road as the residential areas develop. As discussed above connections to the adopted pedestrian and bicycle network can be achieved to allow for industrial workers to walk or bike to work or to the downtown area of Umatilla. There may also be opportunity for future transit connections to the working Kayak system or other transit systems that may be developed.

Conclusion: The proposed subject property is not a mixed-use area. There is potential for alternative modes of transportation to future development on the property through use of walking, biking or public transit such as the CTUIR Kayak.

(9) Notwithstanding section (1) of this rule, a local government may find that an amendment to a zoning map does not significantly affect an existing or planned transportation facility if all of the following requirements are met.

(a) The proposed zoning is consistent with the existing comprehensive plan map designation and the amendment does not change the comprehensive plan map;

(b) The local government has an acknowledged TSP and the proposed zoning is consistent with the TSP; and

(c) The area subject to the zoning map amendment was not exempted from this rule at the time of an urban growth boundary amendment as permitted in OAR 660-024-0020(1)(d), or the area was exempted from this rule but the local government has a subsequently acknowledged TSP amendment that accounted for urbanization of the area.

Applicants Response: This application addresses transportation impacts because these factors cannot be met.

Conclusion: This application addresses transportation impacts because these factors cannot be met.

(10) Notwithstanding sections (1) and (2) of this rule, a local government may amend a functional plan, a comprehensive plan or a land use regulation without applying performance standards related to motor vehicle traffic congestion (e.g. volume to capacity ratio or V/C), delay or travel time if the amendment meets the requirements of subsection (a) of this section. This section does not exempt a proposed amendment from other transportation performance

standards or policies that may apply including, but not limited to, safety for all modes, network connectivity for all modes (e.g. sidewalks, bicycle lanes) and accessibility for freight vehicles of a size and frequency required by the development.

(a) A proposed amendment qualifies for this section if it:

(A) Is a map or text amendment affecting only land entirely within a multimodal mixed-use area (MMA); and

(B) Is consistent with the definition of an MMA and consistent with the function of the MMA as described in the findings designating the MMA.

(b) For the purpose of this rule, “multimodal mixed-use area” or “MMA” means an area:

(A) With a boundary adopted by a local government as provided in subsection (d) or (e) of this section and that has been acknowledged;

(B) Entirely within an urban growth boundary;

(C) With adopted plans and development regulations that allow the uses listed in paragraphs (8)(b)(A) through (C) of this rule and that require new development to be consistent with the characteristics listed in paragraphs (8)(b)(D) through (H) of this rule;

(D) With land use regulations that do not require the provision of off-street parking, or regulations that require lower levels of off-street parking than required in other areas and allow flexibility to meet the parking requirements (e.g. count on-street parking, allow long-term leases, allow shared parking); and

(E) Located in one or more of the categories below:

(i) At least one-quarter mile from any ramp terminal intersection of existing or planned interchanges;

(ii) Within the area of an adopted Interchange Area Management Plan (IAMP) and consistent with the IAMP; or

(iii) Within one-quarter mile of a ramp terminal intersection of an existing or planned interchange if the mainline facility provider has provided written concurrence with the MMA designation as provided in subsection (c) of this section.

(c) When a mainline facility provider reviews an MMA designation as provided in subparagraph (b)(E)(iii) of this section, the provider must consider the factors listed in paragraph (A) of this subsection.

(A) The potential for operational or safety effects to the interchange area and the mainline highway, specifically considering:

(i) Whether the interchange area has a crash rate that is higher than the statewide crash rate for similar facilities;

(ii) Whether the interchange area is in the top ten percent of locations identified by the safety priority index system (SPIS) developed by ODOT; and

(iii) Whether existing or potential future traffic queues on the interchange exit ramps extend onto the mainline highway or the portion of the ramp needed to safely accommodate deceleration.

(B) If there are operational or safety effects as described in paragraph (A) of this subsection, the effects may be addressed by an agreement between the local government and the facility provider regarding traffic management plans favoring traffic movements away from the interchange, particularly those facilitating clearing traffic queues on the interchange exit ramps.

(d) A local government may designate an MMA by adopting an amendment to the comprehensive plan or land use regulations to delineate the boundary following an existing zone, multiple existing zones, an urban renewal area, other existing boundary, or establishing a new boundary. The designation must be accompanied by findings showing how the area meets the definition of an MMA. Designation of an MMA is not subject to the requirements in sections (1) and (2) of this rule.

(e) A local government may designate an MMA on an area where comprehensive plan map designations or land use regulations do not meet the definition, if all of the other elements meet the definition, by concurrently adopting comprehensive plan or land use regulation amendments necessary to meet the definition. Such amendments are not subject to performance standards related to motor vehicle traffic congestion, delay or travel time.

Applicants Response: This proposal is not for a development that would meet the requirements of the MMA. It is for an urban growth boundary expansion and associated change in zoning to Light Industrial.

Conclusion: This application is not subject to requirements of the MMA.

(11) A local government may approve an amendment with partial mitigation as provided in section (2) of this rule if the amendment complies with subsection (a) of this section, the amendment meets the balancing test in subsection (b) of this section, and the local government coordinates as provided in subsection (c) of this section.

(a) The amendment must meet paragraphs (A) and (B) of this subsection or meet paragraph (D) of this subsection.

(A) Create direct benefits in terms of industrial or traded-sector jobs created or retained by limiting uses to industrial or traded-sector industries.

(B) Not allow retail uses, except limited retail incidental to industrial or traded sector development, not to exceed five percent of the net developable area.

(C) For the purpose of this section:

(i) "Industrial" means employment activities generating income from the production, handling or distribution of goods including, but not limited to, manufacturing, assembly, fabrication, processing, storage, logistics, warehousing, importation, distribution and transshipment and research and development.

(ii) "Traded-sector" means industries in which member firms sell their goods or services into markets for which national or international competition exists.

(D) Notwithstanding paragraphs (A) and (B) of this subsection, an amendment complies with subsection (a) if all of the following conditions are met:

(i) The amendment is within a city with a population less than 10,000 and outside of a Metropolitan Planning Organization.

(ii) The amendment would provide land for "Other Employment Use" or "Prime Industrial Land" as those terms are defined in OAR 660-009-0005.

(iii) The amendment is located outside of the Willamette Valley as defined in ORS 215.010.

(E) The provisions of paragraph (D) of this subsection are repealed on January 1,

2017.

(b) A local government may accept partial mitigation only if the local government determines that the benefits outweigh the negative effects on local transportation facilities and the local government receives from the provider of any transportation facility that would be significantly affected written concurrence that the benefits outweigh the negative effects on their transportation facilities. If the amendment significantly affects a state highway, then ODOT must coordinate with the Oregon Business Development Department regarding the economic and job creation benefits of the proposed amendment as defined in subsection (a) of this section. The requirement to obtain concurrence from a provider is satisfied if the local government provides notice as required by subsection (c) of this section and the provider does not respond in writing (either concurring or non-concurring) within forty-five days.

(c) A local government that proposes to use this section must coordinate with Oregon Business Development Department, Department of Land Conservation and Development, area commission on transportation, metropolitan planning organization, and transportation providers and local governments directly impacted by the proposal to allow opportunities for comments on whether the proposed amendment meets the definition of economic development, how it would affect transportation facilities and the adequacy of proposed mitigation. Informal consultation is encouraged throughout the process starting with pre-application meetings. Coordination has the meaning given in ORS 197.015 and Goal 2 and must include notice at least 45 days before the first evidentiary hearing. Notice must include the following:

(A) Proposed amendment.

(B) Proposed mitigating actions from section (2) of this rule.

(C) Analysis and projections of the extent to which the proposed amendment in combination with proposed mitigating actions would fall short of being consistent with the function, capacity, and performance standards of transportation facilities.

(D) Findings showing how the proposed amendment meets the requirements of subsection (a) of this section.

(E) Findings showing that the benefits of the proposed amendment outweigh the negative effects on transportation facilities.

Applicants Response: This request is for the expansion of the City of Umatilla urban growth boundary with an associated request to change the Zoning on the subject property to Light Industrial. Should the City of Umatilla wish to pursue the provisions of this criterion the applicant would be willing to participate. The applicant would assert that the economic benefits of this proposal do outweigh the negative impacts of any transportation impacts that are outlined in the TIA.

Conclusion: Due to the fact that the proposed economic benefits of this application outweigh the negative impacts of transportation impacts. Staff recommendation is to not pursue the provisions of this criterion.

The Umatilla County Comprehensive Plan, Transportation System Plan and Development Code are applicable, specifically Comprehensive Plan Findings and Policies 2, 9 and 25, Transportation System Plan Goals 1 and 3, and Development Code provisions found at 152.019 Traffic Impact Study.

Umatilla County **Comprehensive Plan** Chapter 15. TRANSPORTATION

All segments of Umatilla County's economy depend on the County's transportation network for movement inside County borders and to markets outside of the area. Fortunately, the County and particularly the developing West County has access to five modes of transportation. Interstate and state highways flow east-west and north-south in the County. The Port of Umatilla provides commercial freight use of the Columbia River. Railroad lines including Union Pacific's major switch-yard at Hinkle, bring passenger and freight service to Umatilla County. Two municipal airports make a wide variety of services available to county and regional residents, i.e. agriculture, freight, passenger, business. Natural gas and oil pipelines transport fuel to the county and to other areas. Local traffic between urban areas and highways travels on a fairly extensive county and state roads network. Mass transit is presently limited to long distance commercial bus lines and small fleet bus systems that serves some transportation needs of senior citizens.

The ability of existing services and facilities to serve future regional needs, and the specific requirements necessary to provide balanced forms of transportation for all segments of the county's future population, hinge upon cooperative city/county development of a transportation system plan. A major mechanism insuring this cooperative effort is found within the "Transportation" section of the Joint Management Agreements entered into with all cities of Umatilla County. A Transportation System Plan will also serve to assist state/federal transportation agencies in setting priorities and planning improvements in their areas of responsibilities.

Applicants Response: The following findings and policies are evaluated to meet Umatilla County Comprehensive Plan requirements.

Conclusion: The following findings and policies are evaluated to meet Umatilla County Comprehensive Plan requirements.

Finding 2. Transportation planning within urban growth boundaries is important to insure adequate transportation facilities in the County.

Policy 2. To facilitate transportation system coordination within urban growth boundaries, the cities' TSPs shall apply within the UGB and shall be co-adopted by the County and addressed in the city/county joint management agreements.

Applicants Response: The Joint Management Agreement between Umatilla County and the City of Umatilla is considered as part of this application. Powerline Road is specifically called out in the Joint Management Agreement. There has been a recent transfer of a portion of Powerline Road from Umatilla County to the City of Umatilla. The portion of Powerline Road adjacent to the subject property is still a paved Umatilla County road.

Conclusion: If approved, Powerline road will be adopted by the City of Umatilla down from HWY 730 to the subject property, and be added to the City's TSP. The County co adopted the City's TSP on December 6th, 1999. The TSP was adopted via County Ordinance #99-07.

Finding 9. Many County and public roads are not constructed to an acceptable County standard,

and development is increasing along these roads.

Policy 9. Subdivision of land not on road constructed to County standards or not accepted for maintenance responsibility by the County or state shall not be permitted. A subdivision road shall be public and maintained by a public agency or homeowners association.

Applicants Response: Powerline Road is a paved county road, is classified as a minor collector and is not currently built to that standard. Future development in the subject area would be subject to development standards within the City of Umatilla Zoning Ordinance with appropriate development improvements to Powerline Road with the outcome of bringing the road to the applicable development standard. This will be affected as part of the zone change undertaken by the City of Umatilla once the urban growth boundary expansion is concluded.

Conclusion: Powerline Road is a paved county road, is classified as a minor collector and is not currently built to that standard. Future development in the subject area will be subject to development standards.

Finding 25. The development of I-82 after the County's Comprehensive Plan was acknowledged established new interchanges which could affect the location of industries, commercial businesses and highway-oriented business.

Policy 25A. Examine interchanges and other potential commercial and industrial locations for appropriateness of development taking into consideration access, sewer and water availability and environmental conditions.

Policy 25B. Identify and evaluate factors limiting development in this area.

Applicants Response: The Interstate 82 Powerline Road interchange offers an opportunity to the City of Umatilla to consider additional uses of land between residential areas and the interchange. This application is to expand the City of Umatilla urban growth boundary to allow for additional industrial land to serve data centers, warehousing and certain low impact manufacturing operations. Earlier analysis evaluated these factors, finding the location to be suitable for an urban growth boundary expansion. The associated proposed change in zoning to Light Industrial is compatible with the Interstate 82 Interchange and the adjacent farm uses to the south. The included Umatilla Industrial Area Utility Technical Memorandum indicates that the City of Umatilla does have the capacity to provide services to this area in support of future industrial uses.

Conclusion: The included Umatilla Industrial Area Utility Technical Memorandum indicates that the City of Umatilla does have the capacity to provide services to this area in support of future industrial uses.

The Umatilla County **Transportation System Plan**'s OVERALL TRANSPORTATION GOAL is "To provide and encourage a safe, convenient, and economic transportation system." Goals 1 and 3 are applicable; the appropriate Objectives are addressed here:

Goal 1 Preserve the function, capacity, level of service, and safety of the local streets, county

roads, and state highways.

Objectives

A. Develop access management standards.

F. Develop procedures to minimize impacts to and protect transportation facilities, corridors, or sites during the development review process.

Applicants Response: Upon completion of this urban growth boundary expansion and the zoning of approximately 450 acres for industrial purposes, the City of Umatilla Transportation System Plan and Development Code would be applicable to any development. Those applicable provisions would impose access and development standards meeting this Goal.

Conclusion: Upon approval of the proposed UGB expansion the City of Umatilla's Transportation System Plan and Development Code will be applicable to any development on the subject property. These will fulfil the purposes of this goal.

Goal 3 Improve coordination among the cities of Umatilla County, the Oregon Department of Transportation (ODOT), the US Forest Service (USFS), the Federal Highway Administration (FHWA), and the county.

Objectives

F. Continue to work with cities planning for the county land within their urban growth boundaries.

Applicants Response: The urban growth boundary expansion process is one of cooperation between Umatilla County and the City of Umatilla. Powerline Road, a paved county road, is identified in the Joint Management Agreement for consideration to transfer to the City of Umatilla, a process that was recently completed for a portion of the road north of the proposed action.

Conclusion: The City of Umatilla planning department has involved and informed Umatilla County planning department in preparation of this application. The urban growth boundary expansion process is one of cooperation between Umatilla County and the City of Umatilla. A portion of Powerline road was transferred to the City on June 2, 2020. The City & County will continue to work together as development occurs within the UGB.

Umatilla County **Development Code** provisions 152.019 TRAFFIC IMPACT STUDY.

(A) Purpose: The purpose of this section of the code is to implement Section 660- 012-0045(2)(e) of the State Transportation Planning Rule that requires the County to adopt a process to apply conditions to specified land use proposals in order to minimize adverse impacts to and protect transportation facilities. This section establishes the standards for when a proposal must be reviewed for potential traffic impacts; when a Traffic Impact Analysis must be submitted with an application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; what must be in a Traffic Impact Analysis; and who is qualified to prepare the analysis.

(B) Applicability: A Traffic Impact Analysis shall be required to be submitted to the County with a land use application, when one or more of the following actions apply:

(1) A change in plan amendment designation; or

Applicants Response: A change in plan amendment designation is requested as part of the urban

growth boundary expansion process. A Traffic Impact Analysis is included as part of this application addressing the criteria in these provisions.

Conclusion: A change in plan amendment designation will be completed upon approval. The attached TIA addresses the criteria in these provisions.

(2) The proposal is projected to cause one or more of the following effects, which can be determined by field counts, site observation, traffic impact analysis or study, field measurements, crash history, Institute of Transportation Engineers Trip Generation manual; and information and studies provided by the local reviewing jurisdiction and/or ODOT:

- (a) An increase in site traffic volume generation by 250 Average Daily Trips (ADT) or more (or as required by the County Engineer). The latest edition of the Trip Generation manual, published by the Institute of Transportation Engineers (ITE) shall be used as standards by which to gauge average daily vehicle trips; or
- (b) An increase in use of adjacent gravel surfaced County roads by vehicles exceeding the 10,000-pound gross vehicle weights by 20 vehicles or more per day; or
- (c) The location of the access driveway does not meet minimum intersection sight distance requirements, or is located where vehicles entering or leaving the property are restricted, or vehicles queue or hesitate, creating a safety hazard; or
- (d) A change in internal traffic patterns that may cause safety problems, such as back up onto the highway or traffic crashes in the approach area; or
- (e) Any development proposed within the Umatilla Army Chemical Depot boundary of the I-82/Lamb Road or I84/Army Depot Access Road Interchange Area Management Area prior to the completion of near-term improvements projects (Projects A and B) identified in the I-82/Lamb Road IAMP; or
- (f) For development within the I82/US 730 Interchange Area Management Plan (IAMP) Management Area, the location of the access driveway is inconsistent with the Access Management Plan in Section 7 of the IAMP; or
- (g) For development within the I84/Barnhart Road Interchange Area Management Plan (IAMP) Management Area.

Applicants Response: The completed Traffic Impact Analysis indicates that proposed development on the subject property would decrease pm peak hour traffic by 800 trips as analyzed against the current residential zoning of most of the rezone subject property (please see the earlier analysis). There are impacts to the intersections with both Interstate-82 and Highway 730 during the planning horizon.

Conclusion: The TIA indicates a decrease of pm peak hour traffic by 800 trips. Impacts to the intersections of I-82 and HWY 730 will be addressed at the time of proposed development.

(C) Traffic Impact Analysis Requirements

- (1) Preparation. A Traffic Impact Analysis shall be prepared by a professional engineer. The Traffic Impact Analysis will be paid for by the applicant.
- (2) Transportation Planning Rule Compliance as provided in § 152.751.
- (3) Pre-filing Conference. The applicant will meet with the Umatilla County Public Works Director and Planning Director prior to submitting an application that requires a

Traffic Impact Analysis. The County has the discretion to determine the required elements of the TIA and the level of analysis expected. The County shall also consult the Oregon Department of Transportation (ODOT) on analysis requirements when the site of the proposal is adjacent to or otherwise affects a State roadway.

(4) For development proposed within the Umatilla Army Chemical Depot boundary of the I-82/Lamb Road or I84/Army Depot Access Road Interchange Area Management Plan (IAMP) Management Area Prior to the construction and completion of near-term improvements projects (Projects A and B) identified in the I-82/Lamb Road IAMP, the following additional submittal requirements may be required:

(a) An analysis of typical average daily vehicle trips using the latest edition of the Trip Generation Manual, published by the Institute of Transportation Engineers (ITE) or other data source deemed acceptable by the County Engineer;

(b) A truck and passenger vehicle mode split analysis;

(c) An analysis that shows the traffic conditions of the project at full buildout and occupancy, assuming the background traffic conditions at the year of expected completion;

(d) Findings related to the impacts of the proposed development and the need for Projects A and B to mitigate those impacts. Once Projects A and B have been completed, this Section 4 will no longer apply to new development.

Applicants Response: The included Traffic Impact Analysis, dated May 2020, was completed by J-U-B Engineers, meeting the credential requirements. Umatilla County Development Code provisions at 152.751 are met as this application addresses the transportation requirements in the Umatilla County Comprehensive Plan, Transportation System Plan, and Development Code. Coordination with Umatilla County and the Oregon Department of Transportation was accomplished through consultation with City of Umatilla staff; in-person meetings were limited due to the COVID-19 pandemic.

Conclusion: The TIA meets and addresses the above criterion.

(D) Approval Criteria: When a Traffic Impact Analysis is required; approval of the proposal requires satisfaction of the following criteria:

(1) Traffic Impact Analysis was prepared by an Oregon Registered Professional Engineer qualified to perform traffic engineering analysis;

(2) If the proposed action shall cause a significant effect pursuant to the Transportation Planning Rule, or other traffic hazard or negative impact to a transportation facility, the Traffic Impact Analysis shall include mitigation measures that meet the County's Level-of-Service and/or Volume/Capacity standards and are satisfactory to the County Engineer, and ODOT when applicable; and

(3) The proposed site design and traffic and circulation design and facilities, for all transportation modes, including any mitigation measures, are designed to:

(a) Have the least negative impact on all applicable transportation facilities;

(b) Accommodate and encourage non-motor vehicular modes of transportation to the extent practicable;

(c) Make the most efficient use of land and public facilities as practicable;

(d) Provide the most direct, safe and convenient routes practicable between on-site destinations, and between on-site and off-site destinations; and

(e) Otherwise comply with applicable requirements of the Umatilla County Code.

Applicants Response: The Traffic Impact Analysis was completed by J-U-B Engineers and addresses both Level-of-Service and Volume/Capacity standards. The pm peak hour traffic, when compared with current zoning, is reduced by 800 trips. There are impacts to the intersections at both Interstate-82 and Highway 730 when this action is considered with background growth, creating impacts within the 20-year planning horizon.

Conclusion: Future impacts forecasted by the TIA will be addressed as future development is proposed.

(E) Conditions of Approval: The County may deny, approve, or approve a proposal with appropriate conditions.

(1) Where the existing transportation system is shown to be impacted by the proposed action, dedication of land for streets, transit facilities, sidewalks, bikeways, paths, or accessways may be required to ensure that the transportation system is adequate to handle the additional burden caused by the proposed action.

(2) Where the existing transportation system is shown to be impacted by the proposed action, improvements such as paving, curbing, installation or contribution to traffic signals, construction of sidewalks, bikeways, accessways, paths, or streets that serve the proposed action may be required.

Applicants Response: The applicant request that the County approve this request to expand the urban growth boundary. The Traffic Impact Analysis does show that pm peak hour traffic will be lowered when compared to current zoning. Future development would be subject to City of Umatilla Development Code provisions concerning onsite and adjacent improvements.

Conclusion: The City of Umatilla staff foresee no complications in regards to the approval of the UGB expansion by Umatilla County. Preliminary conversations with the County have not identified any potential issues. Future development would be subject to City of Umatilla Development Code provisions concerning onsite and adjacent improvements.

The City of Umatilla Transportation System Plan, which is a part of the Comprehensive Plan, has certain Goals and Objectives that require review and analysis as well as the Zoning Ordinance Chapter 11 Supplementary Provisions 10-11-10: Traffic Impact Analysis should the transfer of Powerline Road be accomplished prior to the submittal of this application. Additionally, these provisions are applicable to the associated application for a change in Zoning to Light Industrial for the larger subject property. Both the County and City provisions are addressed to assure compliance.

City of Umatilla Comprehensive Plan Chapter 12 Goal 12: Transportation

Section 12:0 Transportation Goal

To develop and encourage a safe, convenient and economic transportation system.

Applicants Response: The applicant supports this overall Transportation Goal of the City of Umatilla. Development of an industrial area on the south side of the City of Umatilla along Powerline Road just north of the Interstate 82 Interchange creates transportation linkages to the larger regional transportation system in a safe and efficient matter, with the opportunity to limit truck traffic within the downtown and residential areas.

Conclusion: The subject property and proposed UGB expansion will allow for development of the Transportation System in a safe and efficient manner.

TSP Goal 1

Promote a balanced, safe, and efficient transportation system.

Objectives

Develop a multi-modal transportation system that avoids reliance upon one form of transportation as well as minimizes energy consumption and air quality impacts.

Protect the qualities of neighborhoods and the community.

Provide for adequate street capacity and optimum efficiency.

Promote adequate transportation linkages between residential, commercial, public, and industrial land uses.

Applicants Response: The applicant would support connection of the proposed industrial area to the residential areas north of the proposal along Powerline Road with appropriate pedestrian and bicycle facilities such as sidewalks or bike lanes. Further connections to downtown Umatilla via the walking bridge or other connections as envisioned in the City's recent trails visioning project are worthwhile.

Conclusion: At the time of future development the Umatilla Development Code provisions will be enforced. Required improvements to adopted City standard at the time development would meet the above criterion.

TSP Goal 2

Ensure the adequacy of the roadway network in terms of function, capacity, level of service, and safety.

Objectives

Identify existing and potential future capacity constraints and develop strategies to address those constraints, including potential intersection improvements, future roadway needs, and future street connections.

Evaluate the need for modifications to and/or the addition of traffic control devices, including evaluation of traffic signal warrants as appropriate.

Provide an acceptable level of service at all intersections in the City, recognizing the rural character of the area.

Applicants Response: The Traffic Impact Study addresses these three Objectives of Goal 2 by evaluating traffic impacts from the proposed urban growth boundary expansion and change in Zoning to Light Industrial. The current growth of residential uses along Powerline Road is creating additional pressure on the Powerline Road intersection with Highway 730 and will over time reduce the Level-of-Service of the intersection. The Traffic Impact Analysis provides an evaluation of traffic impacts along Powerline Road and at the intersections with both Interstate-82 and Highway 730. The Traffic Impact Analysis does provide both timing and the types of improvements that may be appropriate to address future impacts.

Conclusion: The included TIA evaluates in detail the above criterion. The TIA will be consulted for any future development of the subject site. Subsequent development would be subject to City review to ensure the City's roadway network is adequate.

City of Umatilla Title 10 Zoning Ordinance Chapter 11 Supplementary Provisions
10-11-10: TRAFFIC IMPACT ANALYSIS (TIA)

A. Purpose: The purpose of this section of the code is to implement Section 660-012-0045(2)(e) of the State Transportation Planning Rule that requires the City to adopt a process to apply conditions to specified land use proposals in order to minimize adverse impacts to and protect transportation facilities. This section establishes the standards for when a proposal must be reviewed for potential traffic impacts; when a Traffic Impact Analysis must be submitted with an application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; what must be in a Traffic Impact Analysis; and who is qualified to prepare the analysis.

Applicants Response: The applicant has included with this application the Traffic Impact Analysis completed by J-U-B Engineers dated May 2020 meeting these requirements.

Conclusion: The applicant has included with this application the Traffic Impact Analysis completed by J-U-B Engineers dated May 2020 meeting these requirements.

B. Applicability: A Traffic Impact Analysis shall be required to be submitted to the City with a land use application, when the following conditions apply:

1. The application involves one or more of the following actions:

a. A change in zoning or plan amendment designation; or

b. The proposal is projected to cause one or more of the following effects, which can be determined by field counts, site observation, traffic impact analysis or study, field measurements, crash history, Institute of Transportation Engineers Trip Generation manual; and information and studies provided by the local reviewing jurisdiction and/or ODOT:

1) An increase in site traffic volume generation by 250 Average Daily Trips (ADT) or more (or as required by the City Engineer). The latest edition of the Trip Generation manual, published by the Institute of Transportation Engineers (ITE) shall be used as standards by which to gauge average daily vehicle trips; or

2) An increase in use of adjacent streets by vehicles exceeding the 20,000 pound gross vehicle weights by 10 vehicles or more per day; or

3) The location of the access driveway does not meet minimum intersection sight distance requirements, or is located where vehicles entering or leaving the property are restricted, or vehicles queue or hesitate, creating a safety hazard; or

4) The location of the access driveway does not meet the access spacing standard of the roadway on which the driveway is located; or

5) A change in internal traffic patterns that may cause safety problems, such as back up onto the highway or traffic crashes in the approach area.

Applicants Response: The completed Traffic Impact Analysis indicates that proposed development on the subject property would decrease pm peak hour traffic by 800 trips as analyzed against the current residential zoning of most of the rezone subject property (please see the earlier analysis). There are impacts to the intersections with both Interstate-82 and Highway 730 during the planning horizon.

Conclusion: The TIA indicates a decrease of pm peak hour traffic by 800 trips. Impacts to the intersections of I-82 and HWY 730 will be addressed at the time of proposed development.

C. Traffic Impact Analysis Requirements

1. Preparation. A Traffic Impact Analysis shall be prepared by an Oregon Registered Professional Engineer that is qualified to perform traffic engineering analysis and will be paid for by the applicant.
2. Transportation Planning Rule Compliance. See Section 10-13-3 Amendments to the Zoning Text or Map.
3. Pre-application Conference. The applicant will meet with the Umatilla Public Works Director and Planning Director prior to submitting an application that requires a Traffic Impact Analysis. The City has the discretion to determine the required elements of the TIA and the level of analysis expected. The City shall also consult the Oregon Department of Transportation (ODOT) on analysis requirements when the site of the proposal is adjacent to or otherwise affects a State roadway.

Applicants Response: The Traffic Impact Analysis was completed by J-U-B Engineers meeting the qualifications requirement. Section 10-13-3 of the Umatilla Zoning Ordinance is evaluated as part of the associated application for a change in zoning designation to Light Industrial. The applicant and their representatives have met with City staff on several occasions as these applications were being developed.

Conclusion: The TIA was prepared by Shae Talley, an Oregon Registered Professional Engineer meeting the qualifications requirement. Section 10-13-3 of the Umatilla Zoning Ordinance is evaluated as part of the associated application for a change in zoning designation to Light Industrial. City staff has met with the applicant and their representatives on several occasions in preparation for these applications. City staff requested a pre-application meeting with ODOT on June 19th, 2020 and never received any comment due to what staff assume to be Covid-19 delays.

D. Approval Criteria: When a Traffic Impact Analysis is required, approval of the proposal requires satisfaction of the following criteria:

1. Traffic Impact Analysis was prepared by an Oregon Registered Professional Engineer qualified to perform traffic engineering analysis;
2. If the proposed action shall cause a significant effect pursuant to the Transportation Planning Rule, or other traffic hazard or negative impact to a transportation facility, the Traffic Impact Analysis shall include mitigation measures that meet the City's Level-of-Service and/or Volume/Capacity standards and are satisfactory to the City Engineer, and ODOT when applicable; and
3. The proposed site design and traffic and circulation design and facilities, for all transportation modes, including any mitigation measures, are designed to:
 - a. Have the least negative impact on all applicable transportation facilities;
 - b. Accommodate and encourage non-motor vehicular modes of transportation to the extent practicable;
 - c. Make the most efficient use of land and public facilities as practicable;
 - d. Provide the most direct, safe and convenient routes practicable between on-site destinations, and between on-site and off-site destinations; and

e. Otherwise comply with applicable requirements of the City of Umatilla Code.

Applicants Response: The Traffic Impact Analysis was completed by J-U-B Engineers and evaluates the proposed urban growth boundary expansion and associated change in Zoning to Light Industrial with a focus on the impacts to Powerline Road and its associated connections by evaluating both Level-of-Service and the Volume/Capacity standards. The Traffic Impact Study found that the pm peak hour traffic, when compared with current zoning, is reduced by 800 trips. There are impacts to the intersections at both Interstate-82 and Highway 730 when this action is considered with background growth, creating impacts within the 20-year planning horizon.

Conclusion: The TIA indicates a decrease of pm peak hour traffic by 800 trips. Impacts to the intersections of I-82 and HWY 730 will be addressed at the time of proposed development.

E. Conditions of Approval: The City may deny, approve, or approve a proposal with appropriate conditions.

1. Where the existing transportation system is shown to be impacted by the proposed action, dedication of land for streets, transit facilities, sidewalks, bikeways, paths, or accessways may be required to ensure that the transportation system is adequate to handle the additional burden caused by the proposed action.

2. Where the existing transportation system is shown to be impacted by the proposed action, improvements such as paving, curbing, installation or contribution to traffic signals, construction of sidewalks, bikeways, accessways, paths, or streets that serve the proposed action may be required.

Applicants Response: The applicant requests that the City approve this request to expand the urban growth boundary. The Traffic Impact Analysis provides evidence that the proposed change in zoning achieves a lowered pm peak hour by 800 trips at buildout, creating benefits to the operation of Powerline Road. There are impacts to the intersections of Powerline Road with both Interstate-82 and Highway 730 when combined with background growth during the planning horizon. There is opportunity for the industrial area to be connected to the residential area north of the subject property and to the downtown area of the City of Umatilla via sidewalks and bicycle lanes, connecting to the trails network recently adopted by the City Council.

Conclusion: Approval of this application will be determined by the City of Umatilla Planning Commission recommendation and City Council's decision. Staff recommend approval based on findings and conclusions as contained in this report.

Analysis of the Statewide Planning Goals 1 through 14 follows.

Goal 1 Citizen Involvement: To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

Applicants Response: The City of Umatilla Comprehensive Plan and development codes outline the City's citizen involvement program that includes the activities of the Planning Commission and provides for the public hearing process with its required notice provisions. These notice provisions provide for adjoining and affected property owner notice; notice to interested local, state and federal agencies; and allows for public comment to the process.

Conclusion: The required public notice process has been completed and staff hope for citizens to be involved at the Planning Commission and City Council meetings along with any other comments or participation.

Goal 2 Planning: To establish a land use planning process and policy framework as a basis for all decisions and actions related to use of land and to assure an adequate factual base for such decisions and actions.

Applicants Response: Goal 2 establishes the underlining process that a county or a city needs to utilize when considering changes to their Comprehensive Plans and development codes. This application meets those requirements for this request.

Conclusion: Established land use planning processes and policy framework were used in this application.

Goal 3 Agricultural Lands: To preserve and maintain agricultural lands.

Applicants Response: The Goal 3 requires counties to preserve and maintain agricultural lands for farm uses. Counties must inventory agricultural lands and protect them by adopting exclusive farm use zones consistent with Oregon Revised Statute 215.203 et. seq.

Goal 3 does not allow nonfarm uses like industrial development on lands zoned for exclusive farm use unless a local government adopts findings to justify an exception to Goal 3 or accomplishes an expansion of their urban growth boundary. The necessary analysis for an urban growth boundary is set out and included in this application and discusses why this particular location can support a change in designation from Agricultural to Industrial and be included in the City of Umatilla urban growth boundary.

The process the applicant has utilized under Oregon Revised Statute specifically allows an applicant or the community to not consider Goal 3 or Goal 4. The applicant is aware that much of the land surrounding the City of Umatilla is part of the Columbia Valley Viticultural Area as defined in Oregon Revised Statute 195.300 and is therefore consider high-value farmland. While there is significant viticultural development on the north side of the Columbia River in the greater area, at the locations considered as part of this application the aspect of much of the land is not favorable for this type of crop development (not south facing).

Conclusion: The necessary analysis for an urban growth boundary is set out and included in this application and discusses why this particular location can support a change in designation from Agricultural to Industrial and be included in the City of Umatilla urban growth boundary.

Goal 4 Forest Lands: To conserve forest lands by maintaining the forest land base and to protect the state's forest economy by making possible economically efficient forest practices that assure the continuous growing and harvesting of forest tree species as the leading use on forest land consistent with sound management of soil, air, water, and fish and wildlife resources and to provide for recreational opportunities and agriculture.

Applicants Response: There are no forest lands in the City of Umatilla. The community is,

however, a Tree City USA participant, encouraging tree planting to create an urban canopy of trees to provide the many benefits of an urban landscape that includes trees.

Conclusion: There is no forest land in or adjacent to the City of Umatilla.

Goal 5 Open Spaces, Scenic and Historic Areas, and Natural Resources: To protect natural resources and conserve scenic and historic areas and open spaces.

Applicants Response: The subject property does not have any overlays or other known cultural or historical sites. As part of the site analysis earlier in this narrative there was an area that was eliminated from consideration because of the wetlands that are found there. There are no mapped wetlands on the subject property.

Conclusion: The subject site has no inventoried or known features referenced in Goal 5.

Goal 6 Air, Water and Land Resources Quality: To maintain and improve the quality of the air, water and land resources of the state.

Applicants Response: Goal 6 addresses the quality of air, water and land resources. In the context of comprehensive plan amendments, a local government complies with Goal 6 by explaining why it is reasonable to expect that the proposed uses authorized by the plan amendment will be able to satisfy applicable federal and state environmental standards, including air and water quality standards.

The proposed plan amendments do not seek approval of a specific development but seek to apply the City of Umatilla's Light Industrial zoning designation with a specific intent of creating large lot industrial opportunities to serve data centers, transport facilities and manufacturing opportunities. This action can improve air quality by better facilitating the movement of freight along Interstate 82 with connections to Interstate 84 to the south and Highway 730 to the north. Industrial uses at this location will increase impervious surface, although by no more than could have occurred at another location and are subject to environmental requirements imposed by the City of Umatilla and the State of Oregon. The use of construction techniques that include temporary and permanent Best Management Practices for erosion and sediment control and spill control and prevention also can achieve compliance with clean water standards.

Noise is defined as unwanted sound. The uses authorized by the requested plan amendments should not create noise that differs from the types of industrially-related noise that could be comparable to agricultural activities already in the area. The location of these uses in very close proximity to Interstate 82 will reduce overall noise impacts because highway generated noise muffles and obscures other noises located nearby. Open space and landscaping provisions will provide additional protection from noise that may be generated.

Conclusion: As addressed above any negative impacts can be and will be required to be mitigated to the extent possible at time of proposed development.

Goal 7 Areas Subject to Natural Hazards and Disasters: To protect people and property from natural hazards.

Applicants Response: Goal 7 works to address natural hazards and disasters and through a comprehensive plan amendment process would seek to determine if there are known natural hazards and seek to mitigate any concerns. There are no known natural hazards on the subject property, and it is located significantly above and outside the flood plain for both the Umatilla and Columbia Rivers.

Conclusion: There are no known natural hazards on the subject property, and it is located significantly above and outside the flood plain for both the Umatilla and Columbia Rivers.

Goal 8 Recreation Needs: To satisfy the recreational needs of the citizens of the state and visitors and, where appropriate, to provide for the siting of necessary recreational facilities including destination resorts.

Applicants Response: No recreation components are included in this application. However as industrial activities are sited, an increase in tax base for the City of Umatilla would occur. That tax base would provide additional revenue to the City of Umatilla leading to the opportunity for increased investment in parks and recreation opportunities for its citizens and visitors.

Conclusion: The ability to meet Recreation needs will be increased in the City of Umatilla due to the potential increase of the tax base from future development on the subject site.

Goal 9 Economy: To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens.

Applicants Response: Goal 9 requires local governments to adopt comprehensive plans and policies that contribute to a stable and healthy economy. Both Umatilla County and the City of Umatilla have comprehensive plans that have been acknowledged to comply with Goal 9. The City of Umatilla has completed an Economic Opportunities Analysis that is scheduled to be adopted prior to this suite of applications submitted in support of an urban growth boundary expansion, annexation, and change in zoning. The Economic Opportunities Analysis does identify the current inventory of employment lands and recommends adding land to the inventory to accommodate large lot industrial development, meeting the requirement to address a 20-year planning need.

This application is based upon the findings of the October 2019 Economic Opportunities Analysis completed under Goal 9. The major finding of the Analysis was a need for additional large lot industrial land, two opportunities between 50 and 99.9 acres and a third opportunity over 100-acres in size. This application has been done with a focus on data centers, warehousing and light manufacturing. The applicant would assert that adopting the Economic Opportunity Analysis and the update to Goal 9 along with the suite of applications submitted by the applicant would be consistent with Goal 9.

Conclusion: This application is based upon the findings of the October 2019 Economic Opportunities Analysis completed under Goal 9. The major finding of the Analysis was a need for additional large lot industrial land with a focus on data centers, warehousing and light manufacturing. Approving the urban growth boundary expansion would be consistent with Goal 9.

Goal 10 Housing: To provide for the housing needs of citizens of the state.

Applicants Response: Housing is not a specific consideration of this application but is addressed because the associated zone change does propose to rezone just shy of 300 acres of residential land to industrial. Based on the Housing Strategies Report (2019), adopted by the City of Umatilla as part of a Goal 10 update, there is an overabundance of land zoned for single family residential development. The associated application for a change in both Comprehensive Plan and Zoning designations from residential to industrial would not negatively impact the City of Umatilla's needed inventory of residential lands, leaving at least 750 acres over the identified need in the inventory. Please see the attached Housing Strategies Report, particularly the analysis on page 26, that outlines the over 2,100-unit capacity and over 1,000-acre overabundance of residentially zoned land. Removal of 300 acres would not impact the needed residential land supply in the 20-year planning horizon.

Conclusion: Housing is not a specific consideration of this application but is addressed because the associated zone change does propose to rezone just shy of 300 acres of residential land to industrial. Based on the Housing Strategies Report (2019), adopted by the City of Umatilla as part of a Goal 10 update, there is an overabundance of land zoned for single family residential development. The associated application for a change in both Comprehensive Plan and Zoning designations from residential to industrial would not negatively impact the City of Umatilla's needed inventory of residential lands, leaving at least 750 acres over the identified need in the inventory. Please see the attached Housing Strategies Report, particularly the analysis on page 26, that outlines the over 2,100-unit capacity and over 1,000-acre overabundance of residentially zoned land. Removal of 300 acres would not impact the needed residential land supply in the 20-year planning horizon.

Goal 11 Public Services: To plan and develop a timely, orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural development.

Applicants Response: Goal 11 requires local governments to plan and develop a timely, orderly and efficient arrangement of public facilities and services. The goal provides that urban and rural development be guided and supported by types and levels of services appropriate for, but limited to, the needs and requirements of the area to be served. Attached and discussed previously is the Umatilla Industrial Area Utility Technical Memorandum which concludes that the subject area can be adequately served and includes initial cost estimates for consideration.

Conclusion: The UTM addresses bringing public services to the Subject site and determines it to be technically feasible.

Goal 12 Transportation: To provide and encourage a safe, convenient and economic transportation system.

Applicants Response: Goal 12 requires local governments to provide and encourage a safe, convenient, and economic transportation system, implemented through the Transportation Planning Rule. The included Traffic Impact Analysis evaluates the urban growth boundary expansion and related change in designation and zoning based upon the requirements in both the Umatilla County and City of Umatilla Transportation System Plans and Development Codes,

meeting both local and state requirements. Please see the earlier analysis and discussion for specifics or refer to page 17 of the Traffic Impact Analysis for the summary and conclusions. Also included is a comment letter from the Oregon Department of Transportation dated August 21, 2020, signed by Marilyn Holt, District 12 Manager.

Conclusion: As addressed by the TIA and findings in this report the peak PM trips will be decreased by this application. It is reasonable to believe that the subject site will not negatively impact the transportation system in a way that can not be addressed by the findings in the TIA. Needed improvements will be addressed at the time of future development.

Goal 13 Energy: To conserve energy.

Applicants Response: Goal 13 directs local jurisdictions to manage and control land and uses developed on the land to maximize the conservation of all forms of energy, based on sound economic principles. Access to Interstate 82 creates easy connections to Interstate 84, Highway 730 and Highway 395. These connections provide energy efficiency and convenience as travel connections, for both trucks and workers, are easily accessed. It should also be noted that the proposed industrial area is also adjacent to a large and growing residential area with the ability for both pedestrian and bicycle connections creating additional energy conservation opportunities.

Conclusion: The applicants referenced energy conservation opportunities will improve energy conservation in the City of Umatilla.

Goal 14 Urbanization: To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities.

Applicants Response: Goal 14 prohibits urban uses on rural lands. To locate urban uses on rural lands, local governments must either expand their urban growth boundaries to add property or take a Goal 14 exception setting forth reasons why urban development should be allowed on rural lands. This application seeks to expand the City of Umatilla urban growth boundary to allow urban light industrial uses within the city limits. The earlier analysis is in support of an urban growth boundary expansion.

Conclusion: This application seeks to expand the City of Umatilla urban growth boundary to allow urban light industrial uses within City limits. The earlier analysis is in support of an urban growth boundary expansion.

Applicants Conclusion:

In conclusion the applicant encourages the City of Umatilla Planning Commission and City Council, along with the Umatilla County Planning Commission and Board of Commissioners, to approve this request for an urban growth boundary expansion. There are two additional applications submitted to the City of Umatilla for a change in Zoning to Light Industrial and for Annexation of the proposed industrial area. Evidence has been provided in the form of the Economic Opportunities Analysis, Housing and Residential Land Needs Assessment (2019),

Umatilla Industrial Area Utility Technical Memorandum, and Traffic Impact Study to support this and the associated requests. These documents show a clear need for large lot industrial land and indicated that need can be met with city services and without impacts to the transportation system that cannot be mitigated. There is also shown to be no negative impact to the residential land supply leaving a continuing surplus of residential land at approximately 750 acres.

IV. SUMMARY AND RECOMMENDATION

The applicant, Cleaver Land LLC, is proposing to amend the City of Umatilla Comprehensive Plan. Evidence has been provided in the form of the Economic Opportunities Analysis, Umatilla Industrial Area Utility Technical Memorandum, and Traffic Impact Study to support this and the associated requests. These documents show a clear need for large lot industrial land and indicated that need can be met with city services and without impacts to the transportation system that cannot be mitigated. The request appears to meet all of the applicable criteria and standards for this type of request. Therefore, based on the information in Sections I and II of this report, and the above criteria, findings of fact and conclusions addressed in Section III, the City of Umatilla Planning Commission recommends approval of Plan Amendment (PA-2-20).

VI. EXHIBITS

- Exhibit A - Draft Map Change
- Exhibit B - Economic Opportunity Analysis
- Exhibit C - Umatilla Industrial Area Utility Technical Memorandum
- Exhibit D - Traffic Impact Study
- Exhibit E – Supplement Findings
- Exhibit F – DLCDC Comment Letters
- Exhibit G – ODOT Comment Letter



PA-2-20 - Exhibit G

August 21, 2020

VIA EMAIL: Brandon@umatilla-city.org
Brandon Seitz, Community Development Director
City of Umatilla
PO Box 130
Umatilla, OR 97882

Subject: Three applications: Urban Growth Boundary expansion, Annexation, and Zone Change (Plan and Zone Map Amendment) –450 acres for industrial use. Plan and Zoning Map Amendment convert 294 acres of Single Family Residential to Light Industrial.

The Oregon Department of Transportation (ODOT) has reviewed the Urban Growth Boundary (UGB) expansion, Annexation, and Zone Change (Plan and Zone Map Amendment) proposed land use actions. We understand that three applications are submitted as a package/consolidated process. These are for a proposed change of zoning from Single Residential to Industrial along with companion application for an UGB expansion and an annexation of the same property.

ODOT is invested with protecting the safety, operation and function of the state transportation system. For land use proposals subject to the Transportation Planning Rule (OAR 660-012), a Traffic Impact Analysis (TIA) typically is needed to determine whether a development proposal that includes a plan amendment will have a significant effect on transportation facilities along with other information required by local government code and access management rules.

ODOT reviewed the applicant's May 2020 TIA by J-U-B Engineers. Page 17 of the TIA identifies the intersection of Powerline Road/US 730 will need a higher level of traffic control such as a traffic signal or roundabout. Also, both a southbound right-turn lane at the southbound Interstate-82 ramps and a southbound left-turn will be needed at the Interstate-82 northbound ramp.

Accordingly to reflect long-term changes with appropriate improvements, balancing access and circulation management require context sensitive designs to respond to growth. As this area urbanizes, frontage improvements, such as transit facilities, curb, sidewalk, crosswalk ramp(s), bikeways and street standards should be constructed as necessary to provide travel choices and to be consistent with the City's Transportation System Plan (TSP) and ADA standards.

ODOT recommends these elements should be addressed with emphasis on development contributing to implement the improvements that may be necessary to provide safe and acceptable Levels of Service in order to meet City and ODOT standards.

Brandon Seitz, City of Umatilla
Page 2
August 21, 2020

Thank you for the opportunity to comment.

Respectfully,



Marilyn Holt
District 12 Manager

CJS

cc: Ken Patterson, ODOT Region 5 Area Manager
Jeff Wise, PE, ODOT Region 5 Traffic Engineer
Teresa Penninger, ODOT Region 5 Planning Manager
Tamra Mabbott, DLCD

CITY OF UMATILLA, OREGON

AGENDA BILL

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| Agenda Title: First Reading of Ordinance No. 848 - An ordinance amending chapter 9 of the City of Umatilla’s comprehensive land use plan to incorporate the 2019 Economic Opportunity Analysis and 2018 Destination Management Plan. | Meeting Date: 2021-08-02 |
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|---|-----------------------------------|---|----------------------|
| Department: Community Development | Director: Brandon Seitz | Contact Person: Brandon Seitz | Phone Number: |
|---|-----------------------------------|---|----------------------|

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|--------------------------------|---|
| Cost of Proposal: NA | Fund(s) Name and Number(s): N/A |
| Amount Budgeted: NA | |

| | |
|--|------------------------------------|
| Reviewed by Finance Department: No | Previously Presented: NA |
|--|------------------------------------|

Attachments to Agenda Packet Item:

[Ord No. 848.docx](#)

[ORD 848 Exhibit A.pdf](#)

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| Summary Statement: The Council approved City of Umatilla Plan Amendment PA-1-20 at their July 20, 2021, council meeting. Ordinance 848 will implement Plan Amendment PA-1-20 by amending the comprehensive plan. Staff recommends a motion for a first reading of Ordinance No. 848 by title only. A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-1-20) is provided below or available at at City Hall during normal business hours. Umatilla City Council Report and Decision for Plan Amendment (PA-1-20) |
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| Consistent with Council Goals: Goal 2: Promote Economic Development and Job Growth. |
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ORDINANCE NO. 848

AN ORDINANCE AMENDING CHAPTER 9 OF THE CITY OF UMATILLA’S COMPREHENSIVE LAND USE PLAN TO INCORPORATE THE 2019 ECONOMIC OPPORTUNITY ANALYSIS AND 2018 DESTINATION MANAGEMENT PLAN

WHEREAS, the City of Umatilla’s Comprehensive Land Use Plan was codified, reformatted and re-adopted as Ordinance No. 779 on January 8, 2013; and

WHEREAS, The City of Umatilla made application for a comprehensive plan amendment under application PA-1-20 to incorporate the results of the 2019 Economic Opportunities Analysis prepared by Johnson Economics; and

WHEREAS, the Umatilla Planning Commission held a public hearing on August 25, 2020 and September 22, 2020, to consider Plan Amendment PA-1-20 an amendment of Chapter 9 of the City’s Comprehensive Plan, and recommended approval to the Umatilla City Council; and

WHEREAS, the Umatilla City Council conducted a public hearing on July 20, 2021, to consider the Planning Commission’s recommendation for Plan Amendment PA-1-20 and adopted the Planning Commission’s findings and conclusions as its own in approving the application, as contained in the *Umatilla City Council Report and Decision for Plan Amendment PA-1-20*.

NOW THEREFORE, THE CITY OF UMATILLA DOES ORDAIN AS FOLLOWS:

Section 1. Findings. The Umatilla City Council does hereby adopt the findings and conclusions recommended by the City Planning Commission as its own in support of this amendment to the Umatilla Comprehensive Plan, as contained in the *Umatilla City Council Report and Decision for Plan Amendment, PA-1-19*.

Section 2. Comprehensive Plan amendment. The Umatilla City Council does hereby amend Ordinance No 779 by adopting an update to Chapter 9 (Economic Development) of the City of Umatilla’s Comprehensive Plan, as attached hereto as Exhibit A.

Section 3. Severability. If any provision of this Ordinance is held to be invalid by a court of competent jurisdiction, the invalidity shall not affect other provisions of this Ordinance that can be given effect without the invalid provision, and to this end the provisions of this Ordinance are severable.

Section 4. Effective date. The effective date of this Ordinance shall be 30 days after its passage by the city council and approval by the mayor, or by the council president in the mayor’s absence.

ADOPTED by the City Council this ___ day of August, 2021.

Council members voting yes: _____

Council members voting no: _____

Absent Council members: _____

Abstaining Council members: _____

And **SIGNED** by the Mayor this ____ day of August, 2021.

Mary Dedrick, Mayor

ATTEST:

Nanci Sandoval, City Recorder

Chapter 9 of the City of Umatilla Comprehensive Plan will be replaced in its entirety as provided below.

CHAPTER 9 GOAL 9: ECONOMIC DEVELOPMENT

SECTION 9.0 ECONOMIC DEVELOPMENT GOAL

To provide for employment opportunities, revenue generation and economic stability.

SECTION 9.1. ECONOMIC DEVELOPMENT BACKGROUND AND DISCUSSION

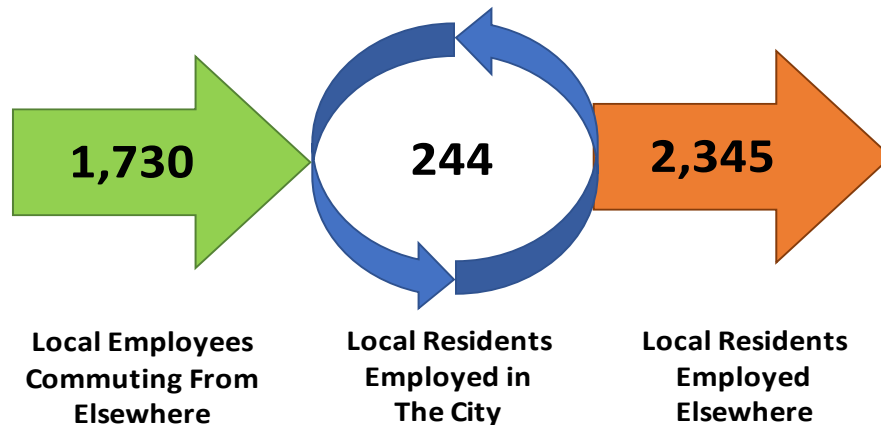
The regional economy has been relatively stable over the past 20 years. Irrigated agriculture and agri-business have been a sustaining economic force, as well as government sector employment (Two-Rivers Correctional Facility, Umatilla School District are the largest public sector employers). More recently, large industrial facilities including data centers and small manufacturing, have helped create a more diverse local economy. Meanwhile, retail and commercial developments have been stagnant.

Commuting has a significant impact on employment and employment opportunities. In the 2019 Employment Lands Analysis conducted for the city, Johnson Economics found that “local residents and employees commute broadly within Umatilla County and beyond. In the City of Umatilla, the local workforce was estimated at roughly 2,589 in 2017, of which 2,345 (90%) travelled outside of the city for employment while an estimated 244 both lived and worked within the city limits.” See Figure 9.1 Below

At the same time, an estimated 1,730 workers commuted into the city for employment, making up over 85% of the local job base of roughly 1,975. (These figures include covered employment and do not capture all forms of self-employment or contracting. Therefore, these figures are best used as an imprecise indicator of the overall pattern.)

This pattern is familiar to many communities across the state, but the extent to which local residents commute elsewhere for employment, and residents of other communities commute in for local jobs, seems somewhat more stark in the case of Umatilla. Commuting patterns are an important element in the local economy. They are indicative of the labor shed from which companies can draw workers, the extent to which job creation translates into increased demand for housing, goods, and services and the overall balance of population and employment in the community.” Johnson Economics, Employment Opportunities Analysis, September 2019.

Figure 9.1



9.1.100 INDUSTRIAL AND JOB GROWTH OPPORTUNITIES

A number of projects are anticipated for Umatilla and the surrounding area. The Umatilla Chemical Depot with approximately 1,000 acres zoned for commercial and industrial use will be available for development as soon as the land is transferred from the United States Army to the Columbia Development Authority. The Port of Umatilla has small and medium sized parcels zoned for industrial development. The Bureau of Indian Affairs owns a 200 acre parcel to the east of Umatilla. Data center development is likely to continue in the region, as well as small manufacturing, food processing and retail and commercial revitalization.

According to the 2019 Johnson Economics report, there are a number of target industries for city, based on the strengths and advantages and established economic development goals. Target industries include data center and cloud storage services, manufacturing (traditional and advanced), tourism and retail, transportation, warehousing and distribution and health care industries.

SECTION 9.2 DOWNTOWN REVITALIZATION AND CIRCULATION PLAN

9.2.010 Introduction

The Downtown Revitalization and Circulation Plan will help the City of Umatilla redevelop the downtown as a vital, pedestrian-oriented center and create an efficient transportation system. The plan identifies a series of projects and strategies that will enhance the pedestrian environment in the community and improve the transportation system.

The plan identifies opportunities for downtown redevelopment, transportation improvements and defines a project implementation strategy, which if properly implemented, should result in significant revitalization of the downtown area. The plan is intended to function as a specific plan with key policies, projects, and programs that guide public and private investment in the community.

9.2.020 *Project Study Area*

The downtown revitalization plan has a general study area defined by the Columbia River to the north, Umatilla River to the south, Powerline Road to the west, and Umatilla River Road to the east. The interchange improvement component of the project will be documented in a separate report and encompasses a study area east of Umatilla River Road to the I-82 interchange with Highway 730. More specific elements of the downtown revitalization plan focused on Highway 730 (6th Street) and one block north to 5th Street and one block south to 7th Street. *Figure 9.2-1* shows the study area.

While plan recommendations are focused on the downtown area of the City, the project study area includes the downtown area and its immediate surroundings. Activities of influence and connections surrounding the downtown were considered to be important factors during the development of the plan.

9.2.030 *Planning Process*

The planning process used to produce the City of Umatilla's Downtown Revitalization Plan is unique in several respects. First, a market analysis was integrated into the study to provide a strategic basis for implementing elements of the Plan. Concepts and design ideas were expressed graphically, to make them more understandable and help the community envision elements of the Plan. The public involvement process was designed to reach a broad segment of the local community and to include people of all age groups in the development of the Plan. During the early stages of the planning process, a broad spectrum of revitalization ideas was considered, but only practical and achievable ideas were included listed as project goals and objectives. Finally, the Plan has been coordinated with the Circulation Plan aimed at alleviating traffic issues associated with the Downtown Revitalization Plan.

More detailed information that resulted from the charrette and public workshops is provided later. Listed below are the major steps included in the planning process and the project meetings that were held.

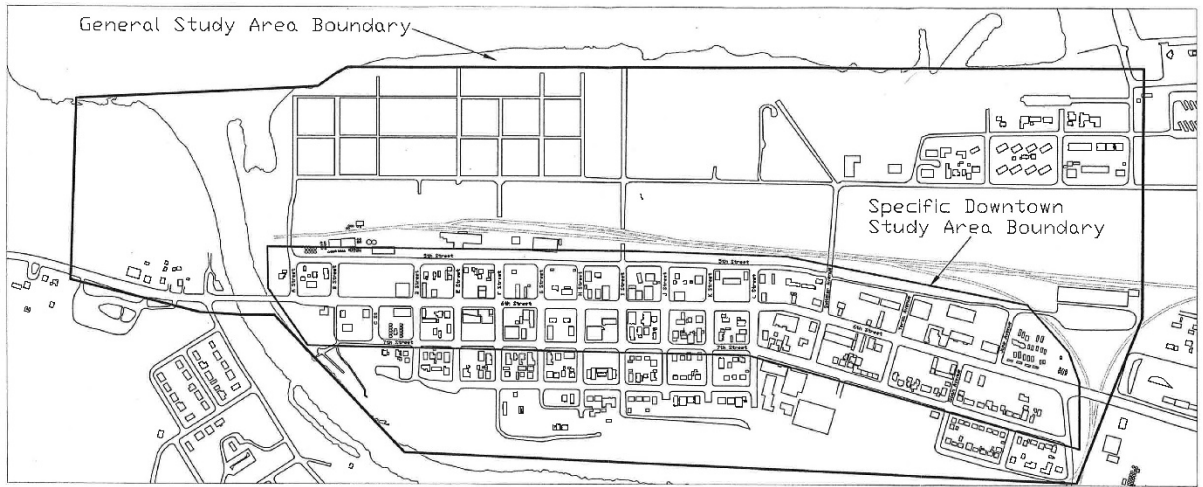
Project Steps

- 1) Project initiation, data collection, meeting preparation
- 2) Project reconnaissance and meetings
- 3) Base map and sections
- 4) Opportunities and Constraints Report
- 5) Community Involvement
- 6) Options for downtown development
- 7) Downtown Development Workshop (consensus)
- 8) Strategic Plan
- 9) Strategic Plan Presentation
- 10) Code amendments – review and adoption

Meetings

October 2000 – Distribute Public Questionnaire

2 November 2000 – Walking Tour and Community Visioning Exercise
2 November 2000 – Management Team Meeting
8 November 2000 - McNary Heights Elementary School Charrette
8 November 2000 – Clara Brownell Middle School Charrette
8 November 2000 – Advisory Committee Meeting
8 November 2000 – Umatilla High School – Adult Charrette
9 November 2000 – Downtown Workshop
22 January 2001 – Dinner Meeting with City Council and Planning Commission
25 January 2001 – Management Team Meeting
25 January 2001 – Interchange Refinement Planning Meeting #1
25 January 2001 – Joint Planning Commission/City Council Work Session
26 April 2001 – Management Team Meeting
26 April 2001 – Interchange Refinement Planning Meeting #2
26 April 2001 – Strategic Plan Presentation to Planning Commission
31 May 2001 – Planning Commission Hearing
4 June 2001 – City Council Hearing



City of Umatilla Downtown
Revitalization and Circulation Study

Figure 9.2-1
Study Area Boundary
Page 50

9.2.100 *OPPORTUNITIES AND CONSTRAINTS*

The planning team determined opportunities and constraints for the downtown revitalization study through site analysis, research, and public involvement techniques. Team findings are documented in detail in the Opportunities and Constraints Report dated June, 2001. Included in this chapter is a summary of the most significant findings of that report.

When developing opportunities and constraints for downtown Umatilla, a wide range of topics was considered. These topics included:

- Questionnaire results and results from the walking tour
- Historic sites
- Inventory and evaluation of pedestrian facilities
- Identification of significant sources of pedestrian traffic
- Location of public buildings and uses
- Analysis of buildings fronting 6th Street
- Inventory of DEQ cleanup sites, sites that store hazardous material and potentially contaminated sites
- Survey of vacant or underutilized land and facilities
- Evaluation of existing neighborhood boundaries and land use
- Street layout and traffic control facilities
- On-street and off-street parking
- Public utilities
- Waterways
- Streetscape
- Pedestrian linkages, and
- Planned public facility improvement projects

After analyzing opportunities and constraints, the team concluded that there does not exist a distinct node or center in downtown around which focus future redevelopments efforts. Alternatively, the downtown is effectively a ‘blank slate,’ allowing great flexibility for future redevelopment.

Table 9.2-1 summarizes the most significant findings of the Opportunities and Constraints report.

The opportunities and constraints were considered in developing the framework for the concept plan. They are also important in determining factors when devising a strategic approach to downtown revitalization.

Because no clearly defined town center exists, members of the planning team, together with community participants, selected a focus area for downtown redevelopment. While about half of the twelve public buildings are scattered in the downtown area, there is a cluster of public uses along Sixth Street between “I” Street and “J” Street. The corner of Sixth and “I” Streets was selected as the focus area for redevelopment and is envisioned as the central place of Umatilla’s

downtown. A long-term opportunity for Umatilla is to cluster public uses near the downtown center.

The opportunities and constraints for downtown Umatilla indicate that immediate results will be difficult to achieve, and that the best results might be realized by implementing a long-term revitalization strategy.

Table 9.2-1. Opportunities and Constraints Summary

| Opportunities | Constraints |
|---|---|
| <ul style="list-style-type: none"> ▪ Schools and the grocery store are pedestrian generators. ▪ Downtown has two nodes of buildings with frontage on Sixth Street. ▪ There is a concentration of buildings around the museum and library. ▪ Downtown vacant and underdeveloped lands offer redevelopment potential. ▪ Downtown has potential linkages to surrounding neighborhoods. ▪ The street grid in downtown provides good access. ▪ Plenty of parking capacity exists in the downtown. ▪ Generally, Sixth Street is well served by improvements such as sidewalks and curbs. ▪ Extension and expansion of the Heritage Trail | <ul style="list-style-type: none"> ▪ There are numerous missing ramps and sidewalks along Sixth Street. ▪ Downtown blocks are underdeveloped and public and private buildings are scattered. ▪ Abandoned gas stations are potentially contaminated sites. ▪ There is no concentration of opportunities in downtown. ▪ Few distinct neighborhoods exist in downtown. ▪ There is no identifiable center in the downtown. ▪ Perceived street crossing problems for pedestrians in downtown. ▪ Side streets lack sidewalks, curbs and other improvements. ▪ The downtown lacks cohesion. |

9.2.200 *MARKET ANALYSIS*

This section provides an overview of the current commercial and residential markets in the City of Umatilla and a brief assessment of the economic opportunities and constraints for revitalizing the downtown. This work recognizes the notable social and economic changes in Western Umatilla County in the last five years. Much of the recent economic activity is related directly or indirectly to the addition of four major employers to the region. Many of the impacts of these employers were anticipated and discussed in the HUES¹ Growth Impact Study, February 1998.

¹ Hermiston, Umatilla, Echo and Stanfield

9.2.210 *Housing Market*

Housing construction in the City of Umatilla has experienced a significant upswing in recent years. The housing needs analysis completed for the City in 1999 identified over 1,200 existing housing units and indicated permitting for new units had been relatively sparse from 1987 through 1998 (approximately 78 permits over 11 years). However, in the last two and half years, 144 new housing units have been permitted (nearly twice that of the previous 11 year period). This trend can be seen from the data summarized in Table 9.2-2.

Recent developments have included Phase 1 of Riverview Estates and Telleron. Phase 1 of Riverview included 49 lots, of which three remain (two with new homes on them).

Table 9.2-2. Housing Permits (1987-2001)

| Unit Type | Prior to 1987 | 1987-1998 | 1999-March 2001 |
|---------------|---------------|-----------|-----------------|
| Single Family | 767 | 68 | 63 |
| Multi-Family | 75 | 8 | 44 |
| Manufactured | 359 | 2 | 37 |
| Total | 1,201 | 78 | 144 |

Note: Adjustment made to 1987-1998 multi-family units permit number to account for reporting variations.

Source: *City of Umatilla Housing Needs Analysis*, Benkendorf Associates Corp., *Oregon Building Permit Reports*, CPRC Portland State University, and City of Umatilla.

Properties were sold both with Hayden Homes and as vacant lots. Twenty-six Hayden homes sold in the last 18 months. The majority of these single-family homes had three bedrooms and two baths. Sales prices ranged from \$78,715 – for a two bedroom two bath home – to \$116,240 – for a four bedroom two bath home. Phase 3 is expected to begin soon.

Eight Telleron properties (which feature Penbrook homes) sold over the same period. Again the majority of the homes included three bedrooms and two baths. Sales prices ranged from \$102,000 to \$164,000.

Other residential development projects include construction of twelve 4-plexes for rental housing, a 265 unit manufactured home park, and conversion of an existing single-family rental community to owner occupied units.

A review of existing and new single-family home sales in the City over the last 18 months indicates a strong market for starter homes priced \$125,000 or less. This data is shown in Table 9.2-3. Of the 96 sales identified, only 6 came in at prices above \$125,000. The least expensive homes were previously owned manufactured units – the majority of which were priced at \$50,000 or less.

Table 9.2-3 City of Umatilla Home Sales for Single Family Units Under \$300,000
(November 1999 to March 2001)

| Sales Price | Unit Type | |
|------------------------|-----------|--------------|
| | Standard | Manufactured |
| Less than \$50,000 | 3 | 10 |
| \$50,000 to \$75,000 | 18 | 3 |
| \$75,001 to \$100,000 | 31 | 2 |
| \$100,001 to \$125,000 | 23 | 0 |
| \$125,001 to \$215,000 | 6 | 0 |
| Total | 81 | 15 |

Note: Sales include existing and new standard housing units and existing manufactured housing resales that include land in price.

Source: Local real estate agent contact.

Table 9.2-4 summarizes the multi-family rent range in the City of Umatilla. Most apartment units in the City were constructed in the 1970s. Multi-family rents range from \$355 to \$700 per month depending on the unit size, furnishings, and/or subsidization. Landlords report that there are many short-term renters associated with temporary employment such as construction projects and agricultural or factory employment. This results in fluctuations in vacancy rates.

With the exception of subsidized housing (which tends to have fewer vacancies), vacancy rates range between 4% and 15%. Landlords comment that there seems to be noticeable excess supply of apartment rentals at the moment.

Table 9.2-4 City of Umatilla Multi-Family Rents

| Unit Size | Rent Range |
|--------------------|----------------|
| Studio & 1 Bedroom | \$355 to \$450 |
| 2 Bedroom | \$410 to \$700 |
| 3 Bedroom | \$470 to \$675 |

Note: Low-end of rent range generally associated with subsidized housing units, some of higher-end units are furnished.

Source: Local property managers.

9.2.220 Commercial Space Market

Currently there is little apparent demand for commercial space in the City of Umatilla. Reportedly no new commercial construction has occurred recently and about half of the recent existing commercial building sales in the City were to the School District.

Commercial rents for the region are summarized in Table 9.2-5. Vacant commercial space is available in both the 6th Street commercial area and in McNary. As a result of the lack of demand, rental rates for commercial space in Umatilla range from \$2.40 to \$6.00 per square foot per year, less than half the cost of space in nearby Hermiston. These rental rates are typically not adequate to support new construction or significant improvements to existing buildings.

Table 9.2-5 Commercial Rents (2001)

| City/Area | Annual Rent per Square Feet |
|-------------------|-----------------------------|
| Hermiston | |
| - Downtown | \$12.00 to \$16.80 |
| - Hermiston Plaza | \$10.00 to \$13.00 |
| - Other | \$10.20 to \$12.00 |
| Umatilla | \$2.40 to \$6.00 |

Note: Detail on condition of commercial spaces not readily available.

Source: Local real estate tenants, appraisers, agents.

Commercial rents in Hermiston reportedly are highest in the downtown (\$12.00 to \$16.80 per SF per year) and drop by as much as 25% (to a range of \$10.00 to \$13.00 per SF per year) for areas outside the downtown. The market for commercial space also appears to be quite price sensitive. An increase in rents at Hermiston Plaza apparently has been accompanied by a loss of tenants including anchor tenant Rite Aid Drugs. Some remaining businesses report a significant drop in sales as a result of this loss and that the quality of incoming businesses and merchandise has lowered.

On the other hand, demand for warehouse or flex space for storage and/or light manufacturing businesses has been noted in both the Umatilla and Hermiston markets.

9.2.230 Economic Assessment

The City of Umatilla has a number of *strengths* but also faces numerous challenges in its attempt to develop a stronger commercial core and city center. The strengths include:

- Successful marketing of port industrial property is bringing in new light industrial and transportation businesses such as trucking, farm tool & implement, and rustic furniture companies.
- Residential development occurring in just the last two plus years has already outpaced development over the previous decade.
- On the demographic side, strong population growth and increasing incomes for households in their prime employment years are being experienced.
- The existing commercial business mix includes some key anchor businesses (such as the grocery, drugstore and bank) and provides a good base from which to build future commercial expansion and investment.

However, despite these strengths, tenant based locally driven commercial development isn't occurring in Umatilla. The following *challenges* are likely reasons for the apparent lack of commercial interest/development:

- The downtown commercial area is a mile-long rectilinear district and is most likely still too large for the number of commercial businesses supportable by the City population and incomes.
- The downtown commercial district is west of both the I-82 freeway and the port of entry.

- Area businesses face direct competition from national and local stores in nearby Hermiston. For at least some forms of retail, the Umatilla trade area is too small to support competing businesses that can generate industry standard sales volumes.
- The downtown commercial area is currently characterized by a significant number of vacant and underutilized commercial properties (including several prominent former gas station sites).
- Based on a series of conversations with local business proprietors, property owners, city and development corporation staff, it appears these groups have a variety of agendas and at times are working against each other – whether intentionally or inadvertently.
- In some instances it has been suggested Umatilla has a poor business climate making entry and start-ups difficult by discouraging potential tenants.
- Commercial rents in Umatilla are not sufficient to support new construction or significant improvement to existing structures.

In summary, there is nothing on the immediate horizon to suggest vacant or underutilized commercial properties in Umatilla will fill absent a program of proactive public/private investment strategies.

9.2.300 *PUBLIC INVOLVEMENT*

The public involvement program for the Downtown Redevelopment Plan involved several components. First, in October 2000, 2900 project newsletters with a community questionnaire were distributed. The second step in the public involvement program was to conduct a walking tour of the study area with local stakeholders. Also included in this tour was an educational presentation of downtown planning concepts and a community visioning exercise. The third step was a series of charettes to generate ideas to revitalize the downtown Umatilla. Three charettes were conducted. Two of the charettes involved elementary and middle school children. The third and final charette was for the general public and held at the Umatilla High School. Following the charettes the next day was a downtown workshop. The downtown workshop summarized the public input and developed alternative concepts to revitalize the downtown area.

9.2.310 *Community Questionnaire*

In October 2000, 2,900 questionnaires were mailed to area residents. The questionnaire was part of a project newsletter and listed a series of open-ended questions. In the six weeks following the mailing, approximately 28 questionnaires were returned. A copy of questionnaire responses is included in Appendix 9A-1. Key observations include the following:

Downtown Identity - Respondents provided a wide variety of answers to a question that asked people to list the landmarks and buildings that best identify downtown. Responses ranged from businesses and buildings in the core area (Carlson’s Drug Store, City Hall, and the Red Apple Cowboy) to outlying uses, such as the Port of Entry and the Umatilla Marina RV Park. Clearly, there was no consensus about what defines downtown Umatilla.

Positive Characteristics - In response to a question regarding the positive characteristics that should be maintained, there were few responses that focused on positive attributes. Some of the positive features included: parks, new high school, craft shops, murals, Umatilla Museum, library, and the MOR Theater.

Negative Characteristics - A longer list of negative characteristics was generated when people were asked to list the features that needed to be improved. Examples included: retail along main street, ugly buildings and signs, trees, green areas, old gas stations, and storm drains.

Commercial Development - In response to a question asking what type of commercial development is desired, people listed the following uses often found in a downtown area: bakery, hardware store, restaurants, a deli, real estate office, professional offices, a bowling alley, clothing store, and shoe store. People also listed uses that are not typically found in downtowns, including a lumber yard, Fred Meyer, Wal-Mart, Bi-Mart, and drive-through restaurants. Again, there was no clear consensus about what should occur in the downtown and what type of businesses should locate there.

Traffic Issues - Public response ranged from general to very specific improvements when asked if there were traffic problems related to pedestrian safety, speeding, parking, and congestion. A number of respondents mentioned the high levels of truck traffic in the downtown and problems for pedestrians attempting to cross Sixth Street.

General Comments - A variety of general comments was provided. Comments addressed issues such as signage, tourism, retail opportunities, and public involvement.

The results of the community questionnaire are included in Appendix 9A-1 with a copy of the questionnaire questions.

9.2.320 *Walking Tour*

On November 2, 2000, the consultant team conducted a walking tour of downtown with a number of downtown stakeholders. As part of that process, a work session was held with participants. Notes from the work session are included in Appendix 9A-2. Key observations included:

Constraints:

- Downtown businesses are too spread out.
- The downtown is a highway with sidewalks.
- There is no reason for visitors to stop in the downtown.
- Hazardous materials at abandoned gas stations limit development.
- There are numerous absentee owners in the downtown.

Opportunities:

- The museum should have a strong presence on Sixth Street.
- Recruit the post office back to the downtown.
- Parking is available on the street.

- Gaps between existing buildings are an opportunity for development.
- “E” Street provides an opportunity to link to the park.
- The tour boats bring many visitors to Umatilla.
- Lots of small improvements will provide a realistic strategy for successful revitalization.

9.2.330 *Charette and Downtown Workshop*

On November 8 and 9, 2000, the H. Lee and Associates team held a two-day charrette that culminated in a downtown workshop attended by stakeholders, city officials, and citizens. The charrette activities on November 8 included two workshops with school children, a meeting with the project advisory committee, and an evening community workshop. On November 9, the consultant team synthesized information collected on the previous day and conducted a downtown workshop in the evening. Two separate reports document the workshop activities. The Charrette Report documents activities on November 8. The Downtown Workshop Report addresses activities on November 9.

9.2.331 *Charrette Report*

Charrette activities conducted on November 8, 2000, are summarized below. Each event is described separately.

Children’s Charrette – McNary Heights Elementary School.



The consultant team met with 16 elementary school children on the morning of November 8, 2000. Before the meeting, teachers had students draw pictures of uses and activities that children envisioned for downtown Umatilla. At the workshop, students took turns describing their drawings. A sampling of student drawings is included in *Figure 9.2-2*. As children talked about their desires, Bob Foster sketched images depicting the type of activities described. Following the discussion, the ideas that were most commonly stated were listed. The students

were then given five adhesive dots and were asked to vote for their favorite ideas. The results are provided in Table 9.2-6 below. Elementary school students had a strong desire to see better parks and playing fields. They also wanted a local swimming pool and the MOR movie theatre reopened. There was interest in creating a science museum. This idea received further discussion later in the day as citizens discussed local natural and archeological features. A sample of the elementary school children design charrette drawings is shown in *Figure 9.2-2*.

Children’s Charrette – Clara Brownell Middle School.

During the afternoon of November 8, 2000, the consultant team met with approximately 10 middle school children. As was done with the elementary school children, students prepared drawings before the workshop. Each child took a turn describing their vision of downtown Umatilla. Again, as students presented ideas, Bob Foster sketched concepts. Common ideas were listed and students voted to identify their preferences. The results are included in



Table 9.2-6 below. Samples of student drawings are shown in *Figure 9.2-3*. The most popular idea among middle school students was establishment of a skating rink - the Skate O’ Rama. Other popular ideas included a swimming pool, movie theater, and skate park.

Table 9.2-6 Summary of Results of Elementary School Children Design Charrette

| Idea of Improvements to Umatilla | <i>Number of Votes for Idea</i> |
|---|---------------------------------|
| Additional Parks and Sports Fields | 17 |
| Swimming Pool | 13 |
| Rehabilitate the Movie Theatre and Open | 13 |
| Mall | 11 |
| Science Museum | 9 |
| Teen Center | 8 |
| Restaurants | 8 |
| Arcade | 5 |
| Skate Park | 4 |

Figure 9.2-2 Sample of Ideas from Elementary School Children Design Charrette



Table 9.2-7 Summary of Results of Middle School Children Design Charrette

| Idea of Improvements to Umatilla | <i>Number of Votes for Idea</i> |
|------------------------------------|---------------------------------|
| Skate o' Rama | 22 |
| Movie Theatre | 9 |
| Swimming Pool | 9 |
| Skate Park | 6 |
| Bike Trails | 3 |
| Bowling Alley | 3 |
| New Middle School | 3 |
| Window Decorations | 2 |
| Fitness Center | 1 |
| Take Down Cowboy and Put Up Viking | 1 |
| More Shops | 1 |

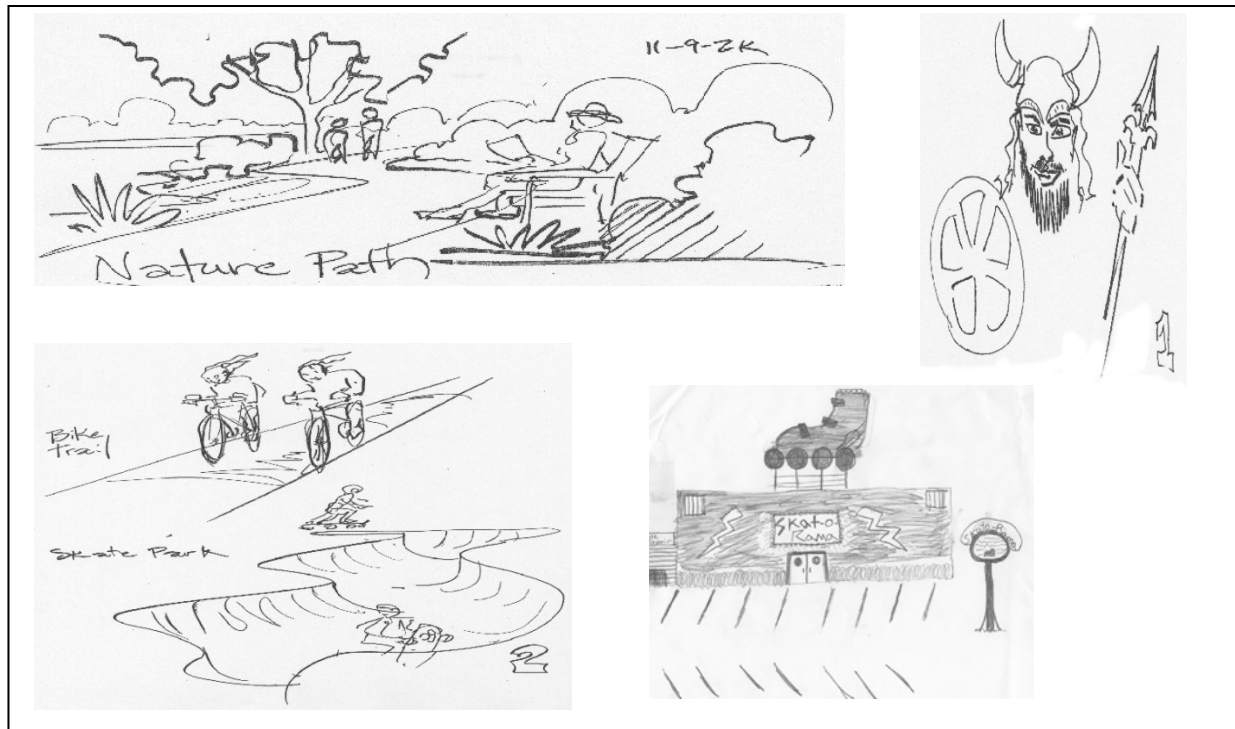
9.2.332 *Project Advisory Committee Meeting*

On November 8, 2000, a project advisory committee meeting was held after the children's workshops and before the evening adult charrette event. The focus of the meeting was to review

activities of the day and identify key concerns of the committee. The following list represents the key concerns of the committee:

- There is a major concern of how traffic will affect the downtown core. Three basic options exist. They are 1) move truck traffic away from downtown via an alternative route, 2) move the downtown center away from 6th Street, and 3) slow traffic down on 6th Street.
- The downtown needs to be separate from the major traffic corridor.
- More commercial development is needed to support the Columbia River tour boats. These developments need to be such as to keep patrons of the tour boats occupied for stops of one to two hours. Ideas for these developments include museums and small shops.
- The tribe should consider developing an interpretive center in town.
- No commercial kitchen exists in town. One should be sought to be developed in conjunction with other retail uses.
- There is a safety concern on 6th Street regarding truck traffic. The preference is to move truck traffic away from the downtown core.
- A traffic light should be considered to be installed along 6th Street to slow truck and general traffic down.
- Timing is critical in any redevelopment of downtown. The major issues are when, where, and how.
- The bridge on the west end of town is in need of replacement. It is tentatively scheduled to be replaced in 2005.

Figure 9.2-3 Sample of Ideas from Middle School Children Design Charrette



9.2.333 *Adult Charrette*

As part of the charrette process, a workshop was held with the general public on the evening of November 8, 2000. The session was held in the Umatilla High School and was attended by approximately 30 people. At the session, the consultant team presented background information on downtown planning principles. The student workshops were described, and the drawings were displayed. Charrette activities included prioritizing goals and providing comments at four workstations. One of the work stations is shown in *Figure 9.2-4*.

Draft downtown goals were prioritized by voting with adhesive dots. Participants were given four dots. Results are presented in Table 9.2-8. Goals that received the most votes were to cluster public and civic uses in downtown and to encourage improvement of downtown buildings. The goals that received the fewest votes were to provide more housing and mixed-use development in the downtown.

Figure 9.2-4 One of Four Workstations at Community Workshop



Four workshop stations were provided. Participants made comments and noted ideas on post-it notes and applied the notes to large aerial photographs. The stations included the following topic areas:

- Urban design, building improvements, and land use.
- Pedestrian improvements.
- Circulation and parking.
- Streetscape improvements.

A transcript of comments from each station is included in Appendix 9A-3. Select comments from the workstations include the following:



Urban Design, Building Improvements, and Land Use.

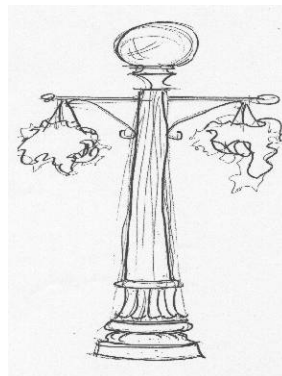
- Murals/green walls
- Awnings
- Signage/sign controls
- Fix up movie theater
- Skate park
- Move post office downtown
- Town garden
- Accent the “cowboy” – light him at night

Table 9.2-8 Downtown Goals

| Downtown Goals |
|--|
| 1. Cluster public and civic uses (Post Office/City Hall) in downtown. |
| 2. Encourage improvement of downtown buildings. |
| 3. Enhance the pedestrian environment by providing a community gathering place and minimizing conflicts between pedestrians and highway traffic. |
| 4. Improve city park (e.g. add a skate park and playing field) near downtown. |
| 5. Define the downtown by creating landmarks, focal points, or other features. Use landscaping, lighting, and street furniture to improve downtown streetscapes. |
| 6. Create a focused core area in the downtown by concentrating new pedestrian oriented development in key locations. |
| 7. Improve pedestrian connections between the downtown and surrounding residential areas. |
| 8. Improve parking in the downtown. |
| 9. Discourage auto-oriented business in the core area of downtown. |
| 10. Provide more housing and mixed-use development in and near the downtown. |

Streetscape Improvements.

- Gateways
- Public art
- Bulb-outs at crosswalks
- Drinking fountains
- Historic pedestrian lighting
- City center gathering spot
- Street trees



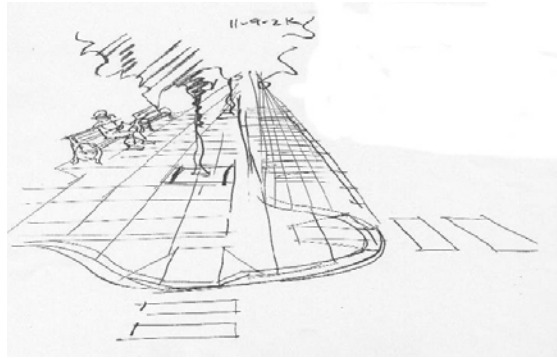
Circulation and Parking.

- Public parking lots
- Parking available on side streets

- Screen parking
- Parking signage
- One-way couplet
- Traffic lights

Pedestrian Improvements.

- Bulb-outs at crosswalks
- Accent paving/raised pavements
- Medians/islands
- Improved sidewalks
- Handicap ramps
- Stop light



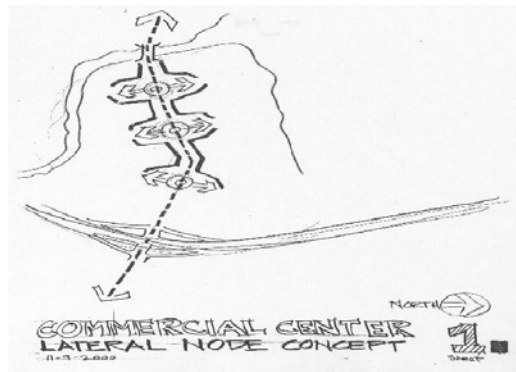
9.2.334 *Downtown Workshop*

On November 9, 2000, a downtown workshop was held at city hall. Approximately 25 people attended the workshop. Participants included stakeholders, city officials, and the general public. The purpose of the workshop was to test a series of downtown alternatives and develop a conceptual framework for development of the downtown plan. The team summarized the work of the previous day and presented four basic concepts for how the downtown could develop including: a nodal commercial center, a truck bypass on Fifth Street, a Seventh Street commercial center, and a new town center. The team presented three variations of the nodal commercial center, which resulted in a total of six alternatives. Each of the six alternatives is discussed below.



9.2.334(1) *Alternative 1. Commercial Center - Lateral Node Concept*

This alternative is intended to focus new pedestrian-oriented commercial development at three nodes along Sixth Street in the downtown area. The nodes are likely to include Switzler Street, I Street, and E Street. The basic nodal concept emphasizes development and pedestrian improvements perpendicular to the highway along the side streets. These streets offer a better, quieter, more pedestrian-friendly environment for people walking or sitting downtown. They also provide linkages to the surrounding residential areas.



9.2.334(2) *Alternative 1A. Commercial Center - Lateral/Civic Center Node Concept*

Alternative 1a is a variation of Alternative 1, but all development is focused on a single node at I Street. This alternative includes the idea of building a new civic center as part of the node. The civic center would include a new city hall and library and would anchor the southern end of the node to the south of Seventh Street. This concentration of activity would help encourage new storefront development along I Street to create a high-quality pedestrian environment perpendicular to the highway. Under this alternative, I Street would become an active gathering place that could be closed at certain times to accommodate events such as a city art fair. This illustration is shown below in *Figure 9.2-5*.

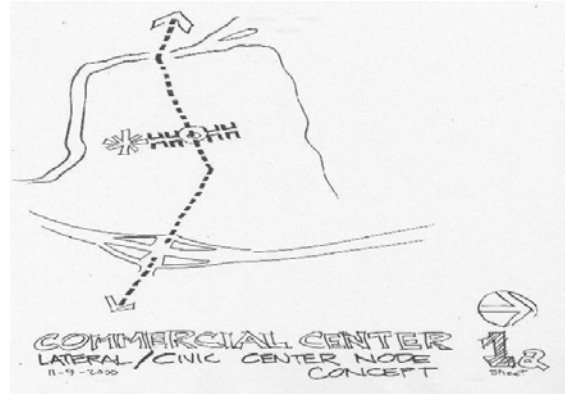
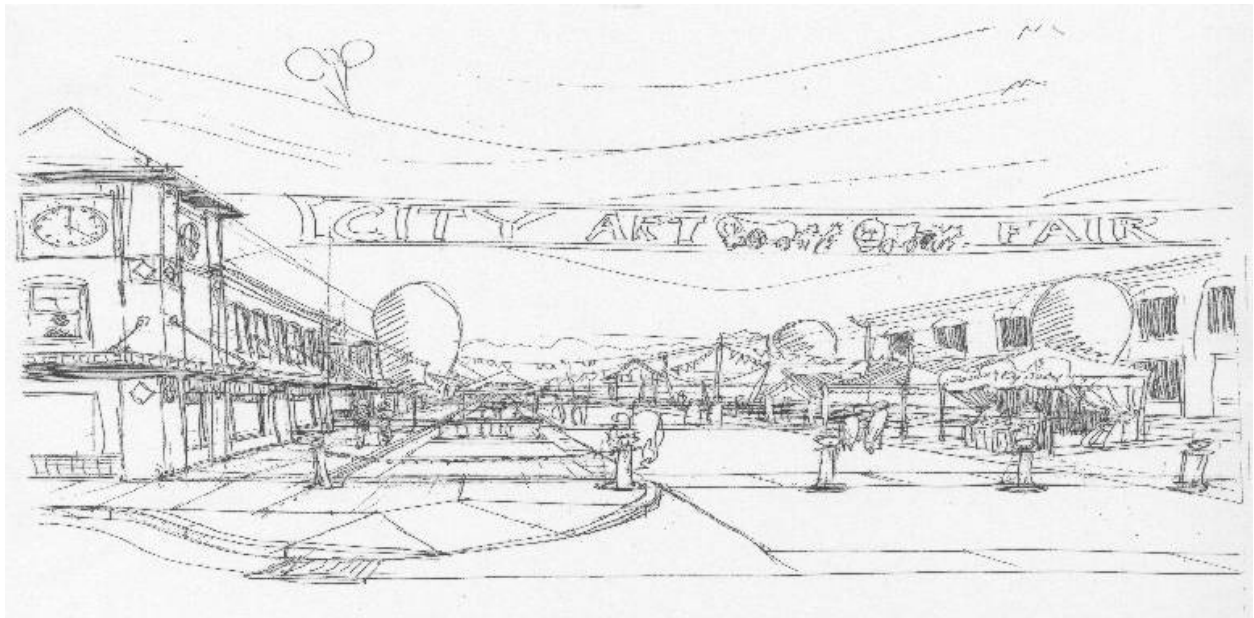


Figure 9.2-5 City Art Fair Concept with Alternative 1A



9.2.334(3) *Alternative 1B. Commercial Center - Linear and Nodal Concept*

Alternative 1b builds on the ideas included in Alternatives 1 and 1a. It emphasizes creation of a pedestrian-friendly shopping area along I Street perpendicular to the highway, but it also emphasizes pedestrian-oriented development between I Street and E Street to take advantage of existing commercial buildings along Sixth Street. The two most distinctive commercial buildings on Sixth Street are the MOR Theater at E Street and the Wards building at I Street. This alternative links the two buildings by encouraging additional development on underdeveloped lots along Sixth Street. The concept is intended to concentrate pedestrian-oriented retail development along a corridor no greater than 1,000 feet in length. The length is critical, because typically shoppers will not walk more than 1,000 feet. Creating a concentration of activity is important if the area is to be a successful pedestrian district. Another feature of this alternative is the location of a new civic center along Sixth Street at the E Street intersection.

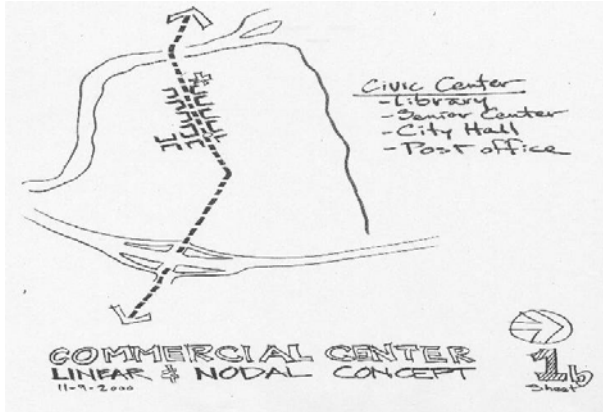
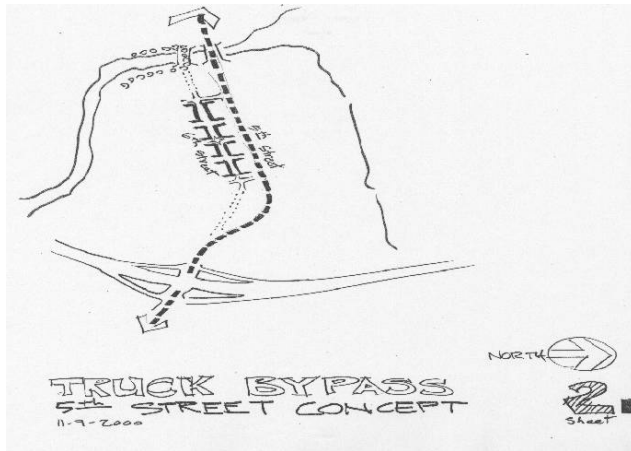


Figure 9.2-6 illustrates a new civic center concept with Alternative 1b.



9.2.334(4) *Alternative 2. Truck Bypass – Fifth Street Concept*

Under Alternative 2, highway traffic would be rerouted along Fifth Street crossing the Umatilla River at a new bridge. The old bridge would continue to provide a pedestrian connection. Downtown development could be enhanced and concentrated by removing the noisy truck traffic from Sixth Street.

Pedestrian-oriented commercial activity would be concentrated to an approximately 1,000-foot-long area along Sixth Street between I Street and E Street.

9.2.334(5) *Alternative 3. Commercial Center Concept - Seventh Street Concept*

Alternative 3 would maintain Sixth Street as an auto-oriented corridor and concentrate pedestrian-focused commercial uses along Seventh Street. This concept was mentioned at previous meetings.

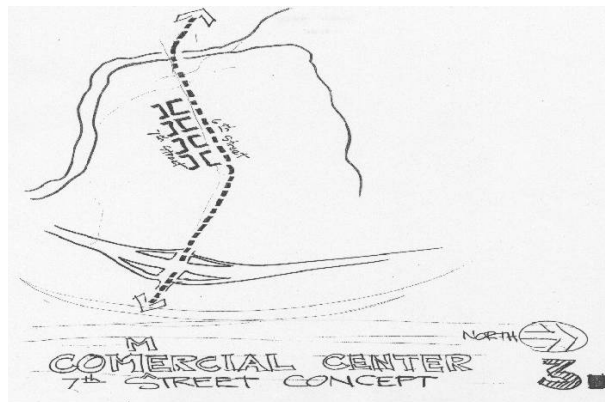
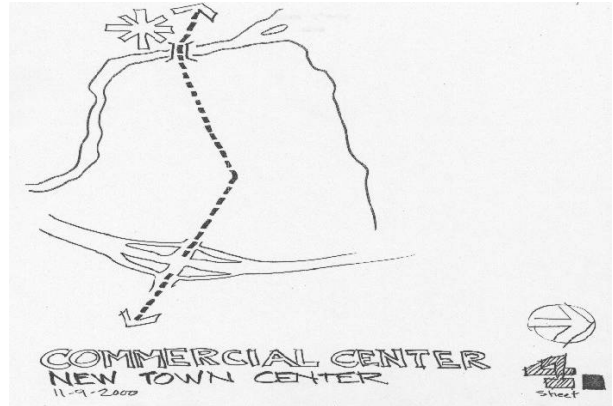


Figure 9.2-6 City Square Concept with Alternative 1B



9.2.334(6) *Alternative 4. Commercial Center - New Town Center*

Like Alternative 3, Alternative 4 would maintain Sixth Street as an auto-oriented corridor. This concept proposes a new pedestrian-oriented town center at a new site west of the Umatilla River.



9.2.335 *Workshop Activity - Prioritize Alternatives*

Following the presentation of the alternatives, workshop participants were given one adhesive dot to vote for their preferred alternative. A second adhesive dot was provided so participants could vote to locate a new civic center. A limited number of participants voted, but of those that did, Alternative 1b was the preferred alternative and I Street was the preferred location for a new civic center.

9.2.336 *Pedestrian and Streetscape Improvements*

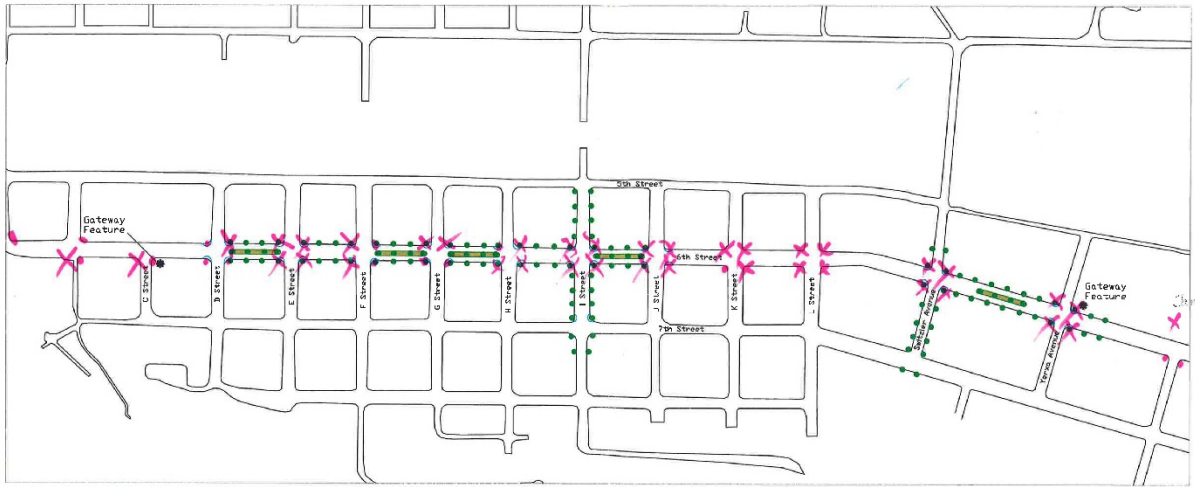
In addition to alternatives, workshop activities focused on pedestrian and streetscape improvements that could be implemented along Sixth Street. Diagrams were used with aerial photographs to identify potential locations for new bulb-out pedestrian crossings, textured crosswalks, medians, and street trees. *Figure 9.2-7* illustrates these diagrams. Participants were generally supportive of all concepts.

9.2.337 *Land Use Districts*

New land use districts were identified for the downtown. A key element of the district concept is the downtown core area that extends from D Street to I Street. In the downtown core, pedestrian-oriented retail and service development would be encouraged, and auto-oriented uses would be discouraged. To the east and west of the downtown core along the highway, a “downtown transition district” would be formed that would allow some auto-oriented commercial uses. Flanking the downtown would be districts that would allow higher density housing. New housing in the downtown can help enliven the area and bring more customers to retail businesses. *Figure 9.2-8* shows the land use district map described above.

9.2.338 *Conclusion*

The two-day charrette and workshop process produced a wealth of ideas for the downtown. These ranged from large-scale projects, such as a science museum, to simple projects, such as improved flower boxes. The key to success for Umatilla will be to identify projects and programs that are realistic and can truly be implemented, given the financial capability of the community. A concept plan was developed after the downtown workshop. This concept plan is shown in *Figure 9.2-9*.



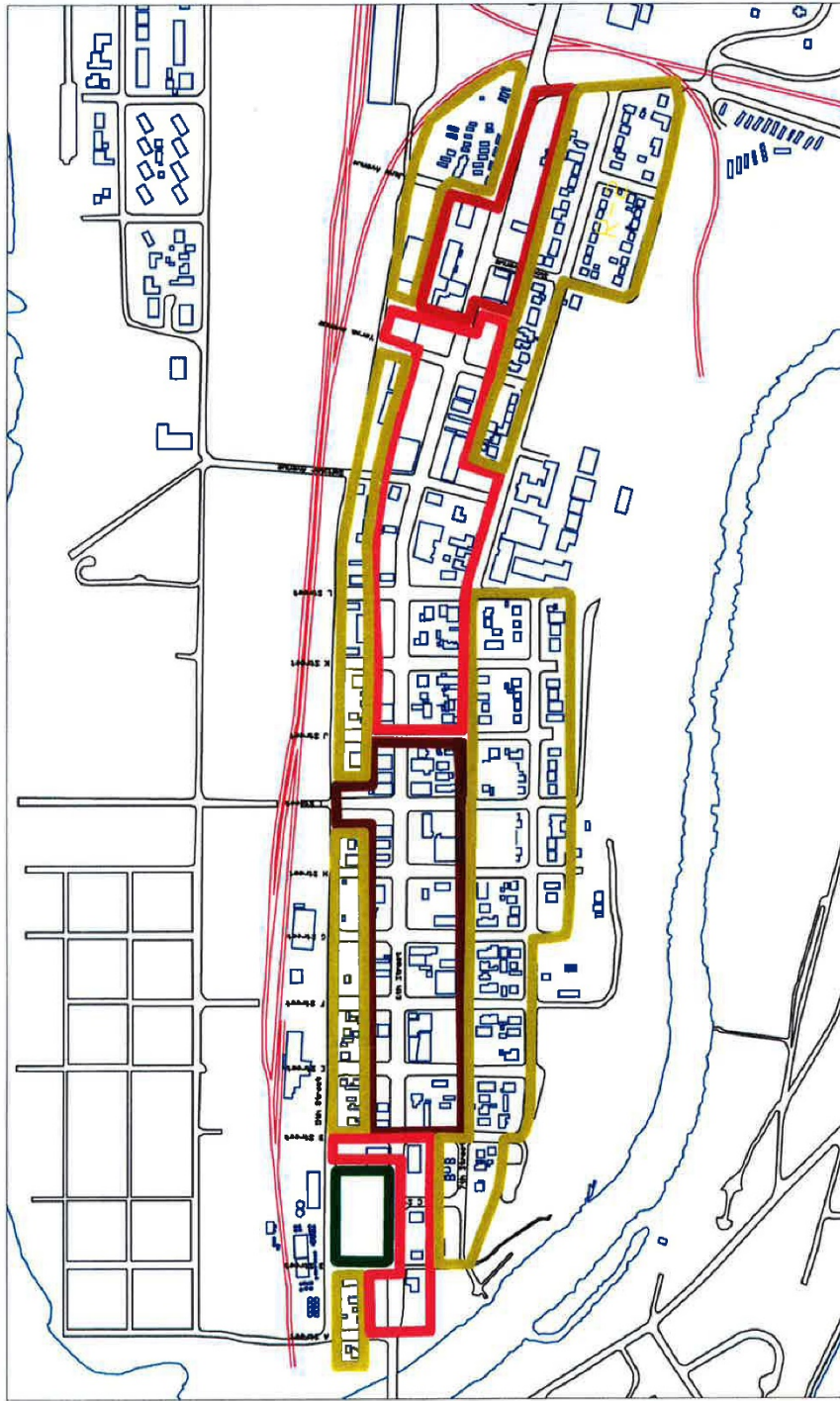
LEGEND

- Bulbouts
- Street Trees
- Median



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Figure 9.2-7 Pedestrian and
 Streetscape Improvements
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- LEGEND**
- ▬ Commercial
 - ▬ Residential Cemetery
 - ▬ Downtown Core
 - ▬ Downtown Transition

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 As amended by Ordinance #731

Figure 9.2-8 Land Use District Map
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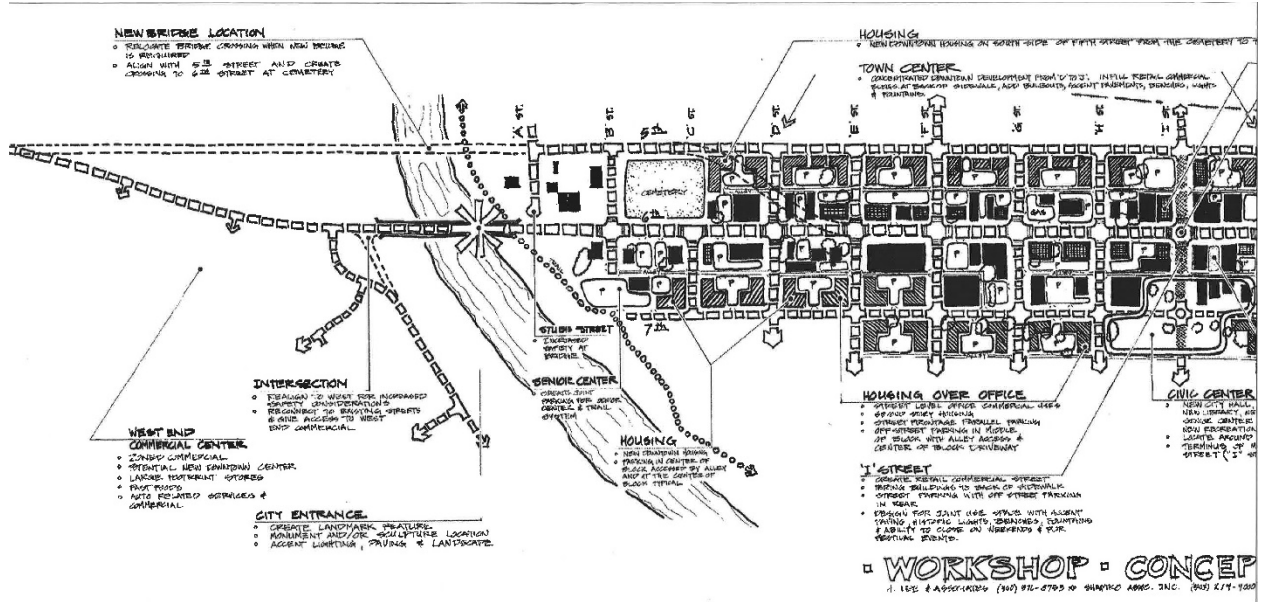


Figure 9.2-9 (1 of 2)

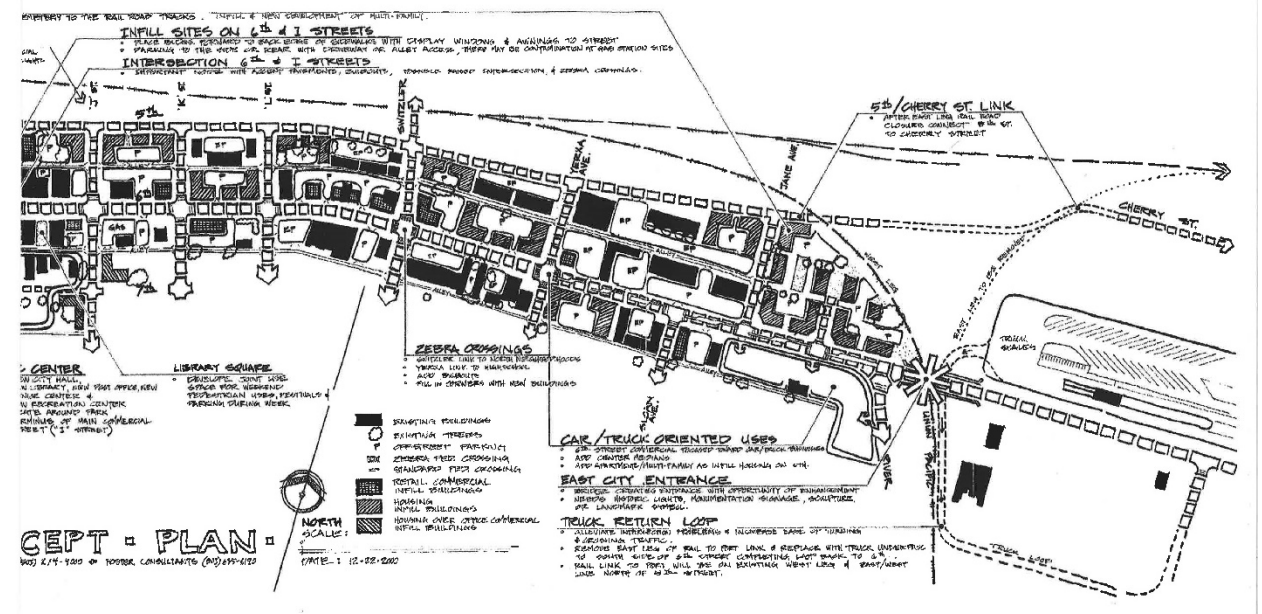


Figure 9.2-9 (2 of 2)

9.2.400 *DOWNTOWN REVITALIZATION PLAN*

The purpose of the project is to create a strategic plan for the revitalization of the downtown core area of the City. The plan is intended to enable the community to benefit from an improved and efficient transportation system and redevelopment of the downtown core area as a vital, pedestrian-oriented center.

Objectives of the Downtown Revitalization Plan are to:

- Create incentives for businesses to locate in the core area of the City;
- Create a pleasant pedestrian experience in the core area;
- Identify streetscape improvements that will enhance the character of the City, including sidewalk treatments and lighting.
- Mitigate the effects of truck traffic along U.S. Route 730; and
- Identify redevelopment opportunities.

The greatest challenge for the City is to find a way to attract significant private investment in the downtown over a long period of time. The revitalization plan is a blueprint to achieve this goal.

One of the guiding principles of the Downtown Revitalization Plan is to concentrate redevelopment efforts. A key problem with the existing pattern of development is that it is scattered over a large area. By first focusing the community's efforts on a small area, the City can later expand the area of focus to other locations throughout downtown. The intent of concentrating redevelopment efforts is to achieve a critical mass of business activity and investment that will trigger a cycle of economic growth.

Commercial opportunities are often present at intersections that have high rates of flow of either vehicles or pedestrians. The intersection of U.S. Route 730 and Interstate 82 is the primary vehicular intersection in the City. During the planning workshop, members of the community identified the primary pedestrian intersection in downtown as the junction of 6th Street (U.S. Route 730) and I Street. The Downtown Revitalization Plan recommends that the City focus on the potential of the 6th Street/I Street intersection. The US Route 730 and Interstate 82 intersection improvements are a part of the Interchange Refinement report that is a separate report.

Public improvements and redevelopment can help attract new businesses. Because downtown Umatilla lacks sufficient investment to attract other commercial activities, the City should commit public resources to improvements and redevelopment that make the downtown a viable and attractive location for businesses. In addition to investing in civic improvements, the City should also attempt to concentrate jobs, commercial activities, and housing in the downtown.

9.2.410 *Redevelopment Opportunities*

This section presents a summary of redevelopment opportunities envisioned to help revitalize downtown Umatilla. Together, these opportunities establish a conceptual framework for redevelopment that is consistent with the vision and goals of the community identified during the planning process.

9.2.410(1) *Town Center*

Town Center is envisioned as a pedestrian friendly district. To help concentrate future development and make the area pedestrian friendly, a 1,000-foot distance was used to create alternative development concepts for downtown Umatilla. This is a comfortable walking distance for most pedestrians.

The Downtown Revitalization Plan proposes that future development be concentrated along 6th Street between E and J streets. Town Center also would extend along I Street one block to the north of 6th Street and along I Street to the south; a site for a proposed Civic Center. Retail and commercial infill projects would be the most desirable type of development for Town Center. New structures would typically include commercial storefronts that abut the sidewalk.

Pedestrian and streetscape improvement projects, including street furnishings such as benches, lights and fountains, would be focused in the Town Center. To improve the pedestrian environment, sidewalk bulb-outs would be added at critical intersections along 6th Street, as well as crosswalks and accent pavements.

9.2.410(2) *Intersection of 6th and I Streets*

The Downtown Revitalization Plan identifies the intersection of 6th and I streets as potentially the strongest pedestrian node within the community. The intersection is the commercial center for the downtown area because 6th Street is the primary vehicular route through town and I Street is considered to have the greatest potential as a pedestrian-oriented cross street. Improvements to this intersection would include accent pavement, benches, and possibly a raised intersection with pedestrian crossings designated with wide crossing strips.

Two infill sites are adjacent to this intersection, one on the northwest corner, the other on the southwest corner. An underutilized property also is identified at the southeast corner of the intersection. As the revitalization plan is implemented, redevelopment of these properties is likely. New structures at these locations should follow the commercial storefront model by building to the edge of the sidewalks and having facades orienting toward both 6th and I streets, including display windows and awnings.

Figure 9.2-10 illustrates a vision of what 6th Street east of I Street could look like with the improvements described above.

Figure 9.2-10 6th Street Future Concept with Streetscape and Building Improvements



9.2.410(3) *I Street*

Participants at the community workshops identified I Street as a likely place for a pedestrian center. I Street should be redeveloped as a commercial street strongly oriented to pedestrians. Commercial storefronts would abut the sidewalk. Street parking can be supplemented by off-street parking located behind buildings that face I Street. The street would be designed for both vehicle and pedestrian use by using accent paving, historic lights, benches and fountains. As a pedestrian street, I Street could be closed to vehicular traffic for special events or festivals.

9.2.410(4) *MOR Theatre*

One of the most significant buildings located in downtown is the MOR Theatre. While some community participants commented on the building's poor appearance, others suggested that the theatre was worth keeping. Renovation of the building may serve both of these ends. *Figure 9.2-11* illustrates how the exterior of the building might look after a major renovation. It should be noted that building renovation is typically more expensive than new construction. Major renovation of existing structures may not be economically viable until the revitalization of downtown is well underway.

9.2.410(5) *First Interstate Bank Building*

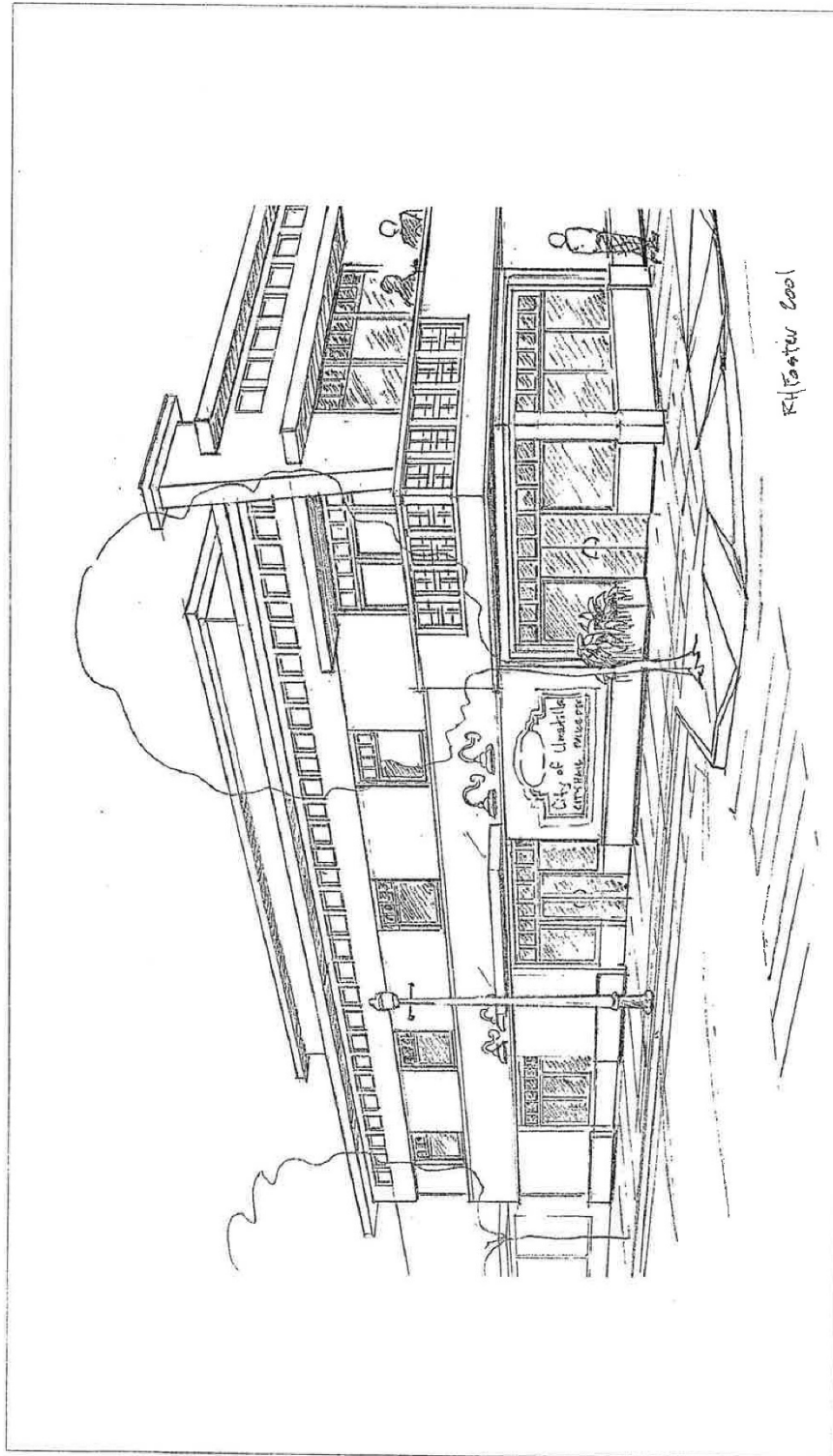
The First Interstate Bank building, which now houses the school district offices, is of relatively new construction. The building is a utilitarian structure that lacks the detail and architectural character that are appropriate for a pedestrian-oriented downtown. *Figure 9.2-12* shows a sketch illustrating how architectural detailing applied to the building's exterior might enhance its appeal to pedestrians in a manner suitable for its downtown location.

9.2.410(6) *Civic Center*

To anchor I Street as the primary pedestrian axis, the plan envisions a Civic Center located at the intersection of 7th and I Streets. This complex of buildings would include a new city hall, library, post office, senior center, and recreation center. The plan presents two options for development of the Civic Center. One concept closes 7th Street west of the intersection of I Street and creates a Civic Center complex with new buildings oriented toward a park. Option 1 is illustrated in *Figure 9.2-13*. The second concept closes I Street immediately south of 7th Street and centers the Civic Center complex and park on the axis of I Street. Option 2 is illustrated in *Figure 9.2-14*.

9.2.410(7) *Library Square*

The plan identifies an open space located between the library and 6th Street that might become an attractive pedestrian gathering place. The plan proposes that this space be developed as a joint-use space that allows parking during the week, but is available for pedestrian use on weekends. The Library Square project would likely reduce the number of existing parking spaces, planting shade trees and landscaping, and street furnishings such as benches, streetlights, and bollards. *Figure 9.2-15* shows an improvement concept for the existing library parking lot where the area is accented with an archway façade and pedestrian plaza.



MOR Theatre Renovation Concept
Figure 9.2-11

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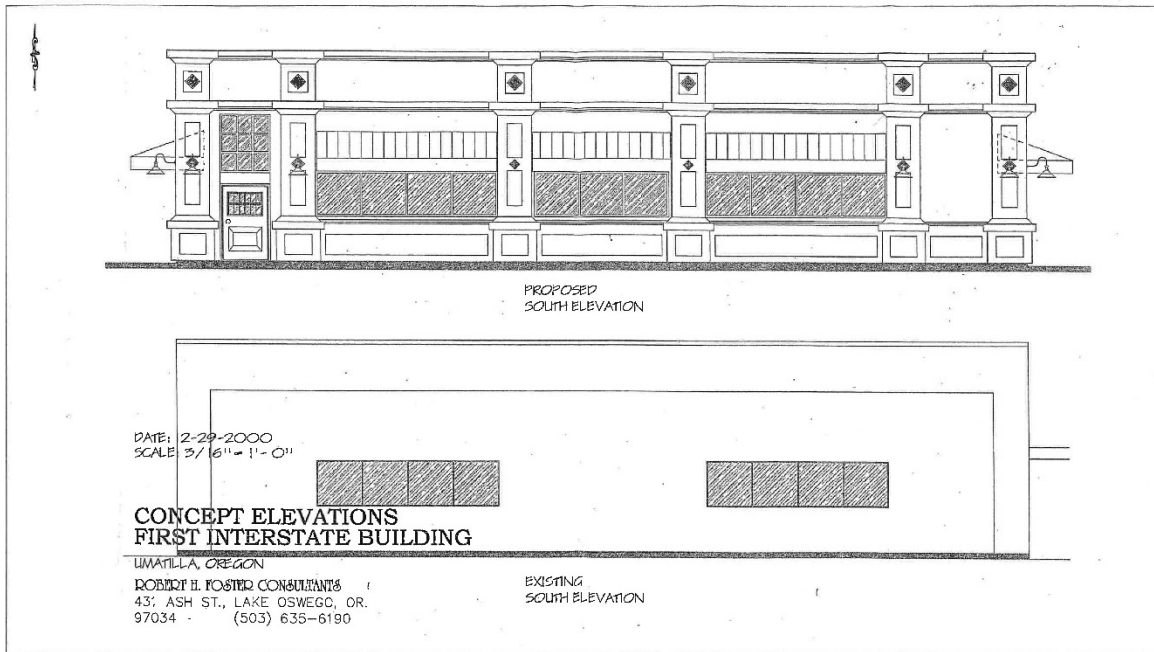


Figure 9.2-12

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First Interstate
Building Renovation Concept Page 77

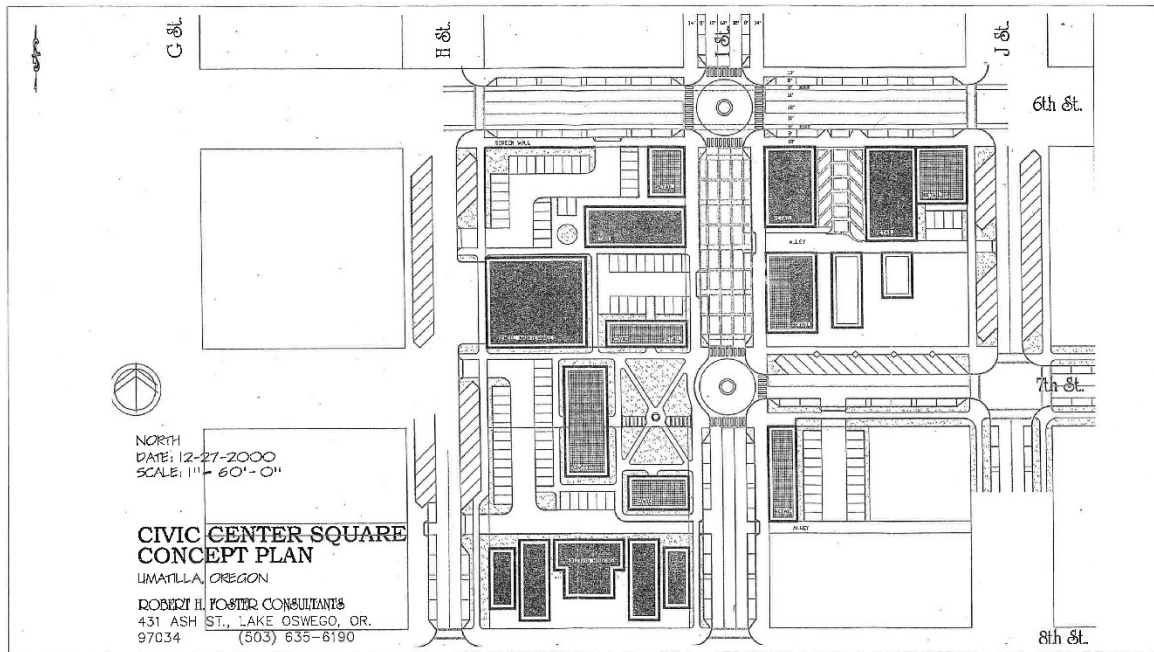
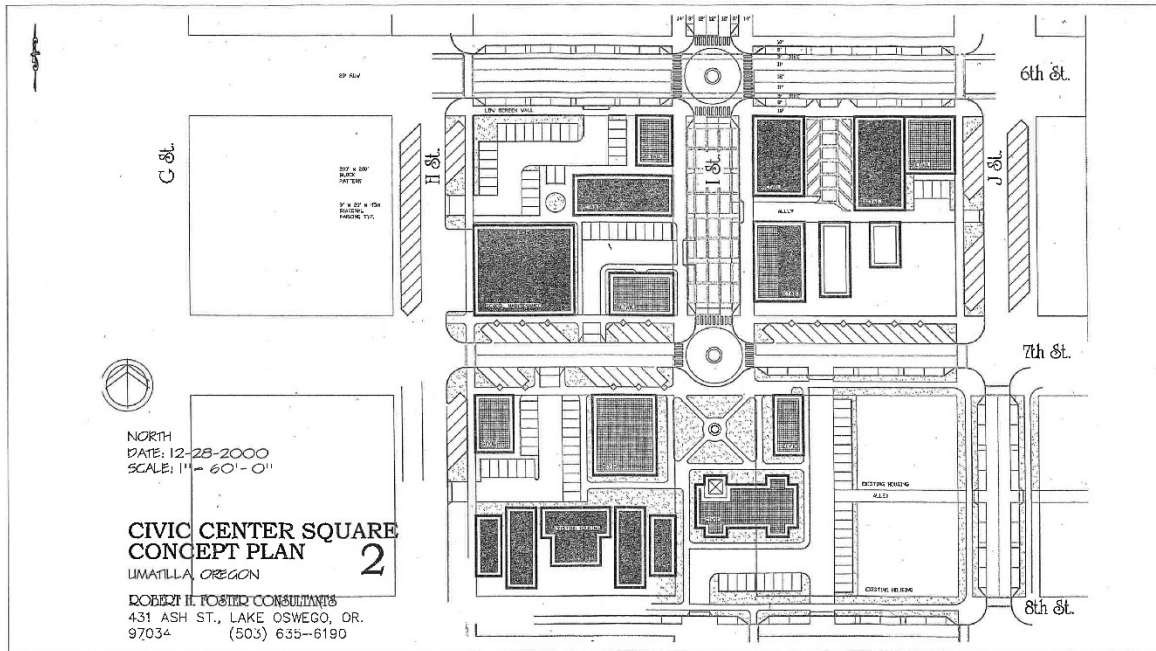


Figure 9.2-13

Civic Center
Concept - Option 1

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City of Umatilla Downtown
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Figure 9.2-14
 Civic Center
 Concept - Option 2

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Figure 9.2-15. Library Plaza Concept with Archway Façade



9.2.410(8) *Downtown Residential*

Creating residential housing opportunities downtown will help the City establish a stronger downtown neighborhood. New residents will build a market for new downtown businesses. Downtown housing development should be oriented toward the street with parking in the center of the block. Access to center block parking lots would be by alleys running east to west between the numbered streets. New housing development would be encouraged to follow this typical pattern of development.

9.2.410(9) *Downtown Mixed-Use*

The Downtown Revitalization Plan proposes a new development pattern for downtown Umatilla that combines office and residential uses. This new pattern would allow office and commercial uses at street level with housing on the second floor or behind the storefront. Parking would be available on the street with off-street parking located in the middle of the block. Access to mid-block parking would be by alleys running east to west between the numbered streets and driveways located at the center of the block.

This mixed-use development concept is illustrated with typical plan and section drawings in *Figure 9.2-16*. The plan and section drawings illustrate how retail or office space might be located on the first floor with housing on the second floor. The drawings also illustrate how off-street parking can be included in this type of development.

9.2.410(10) *Auto-Oriented Commercial*

A portion of the east side of downtown Umatilla is suitable for car- and truck-oriented uses. The plan proposes that truck- and car-oriented uses be located along 6th Street from a point west of the intersection of Yerxa Avenue running east to the Union Pacific Railroad bridge. Given that this location is near the Port of Entry and the Interstate 82 interchange, it provides an opportunity for commercial development in this area.

9.2.420 *Streetscape and Pedestrian Improvements*

This section describes streetscape and pedestrian improvements that are a part of the Downtown Revitalization Plan. Sketches were developed to depict streetscape design elements, based on “typical” conditions. Recommended improvements have been broken down into illustrative projects, which include estimated project costs the City can use for the purposes of planning.

9.2.421 *Design Elements*

To develop a schematic streetscape plan, it was assumed that a typical block measured 200 feet by 200 feet and that the typical street right of way measured 80 feet in width, with streets measuring 60 feet from curb to curb. The existing 6th Street cross section is illustrated in *Figure 9.2-17*. The section view shows an 8-foot-wide parking lane on both sides of the street, one 15-foot-wide travel lane in each direction and a 14-foot-wide center turning lane.

The proposed 6th Street cross section design reduces the width of the travel lanes and adds center medians to slow traffic. Bike lanes have also been designated. The proposed 6th Street section, see *Figure 9.2-18*, shows a 7-foot-wide parking lane on both sides of the street, one 6-foot-wide

bike lane in each direction, one 11-foot-wide travel lane in each direction, and a 12-foot-wide center turning lane or median.

A concept plan was developed for the 6th and I Streets intersection. I Street should become the City's "Pedestrian Street." Sixth Street is already considered to be the City's "Main Street." The concept plan shows curb extensions and crosswalks, 10-foot-wide sidewalks along 6th Street, and 14-foot-wide sidewalks along I Street. Each curb extension depicts ramps to meet requirements of the American's with Disabilities Act (ADA) and planting areas for landscape plants or trees. The concept plan also depicts concrete scoring patterns used on the paving surface. The 6th Street and I Street intersection concept plan is shown in *Figure 9.2-19*.

A "typical" sidewalk pavement sketch in *Figure 9.2-20* illustrates the sidewalk design proposed for new improvement projects. The sketch depicts a sidewalk that varies from 8- to 10-feet wide, curb and gutter, street trees, and parking. A second sketch shown in *Figure 9.2-21* illustrates the need for added width to include streetscape furnishings and other amenities. This sketch defines a 4-foot wide furniture zone adjacent to the curb where street trees, lights, signage, benches, trashcans, and drinking fountains should be located. The sketch also shows how storefront awnings (from 4- to 6-feet-wide) can make the street more attractive to pedestrians.

A new proposed local side street cross section in the downtown core area is shown in *Figure 5-13*. This cross section shows angle parking on both sides of the roadway and two travel lanes within a pavement width of 64 feet and 80-foot right of way. The angle parking lanes each take 20 feet and the two travel lanes are 12 feet each. The remaining right of way is comprised of 6 foot sidewalks. An additional two feet of right-of-way is available for utilities behind the sidewalks on each side of the street.

9.2.422 *Illustrative Improvement Projects*

Using the design concepts described above, a list of illustrative improvement projects was developed to help the City budget and plan for implementation of the pedestrian and streetscape improvements (see the Pedestrian and Streetscape Improvement Plan, *Figure 9.2-7*). Recognizing that financial resources to implement the improvements might be limited, a large number of small projects were created. The series of cost estimates for typical improvements were used to develop the costs for illustrative improvement projects. The cost estimates are intended to be used by the City for planning and budgeting purposes and are not intended to reflect the actual cost of the improvements. A sequence of small projects allows the City greater flexibility in implementing the improvements. If financial resources are limited, fewer projects can be implemented. If funding is readily available, then the improvement schedule can be accelerated.

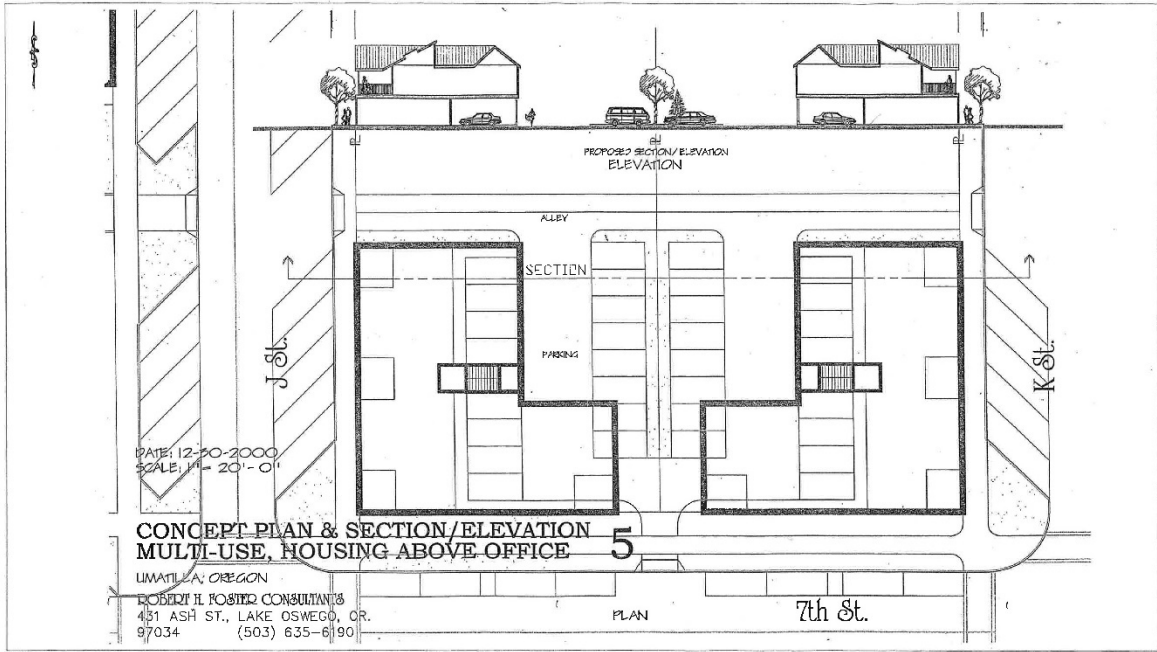
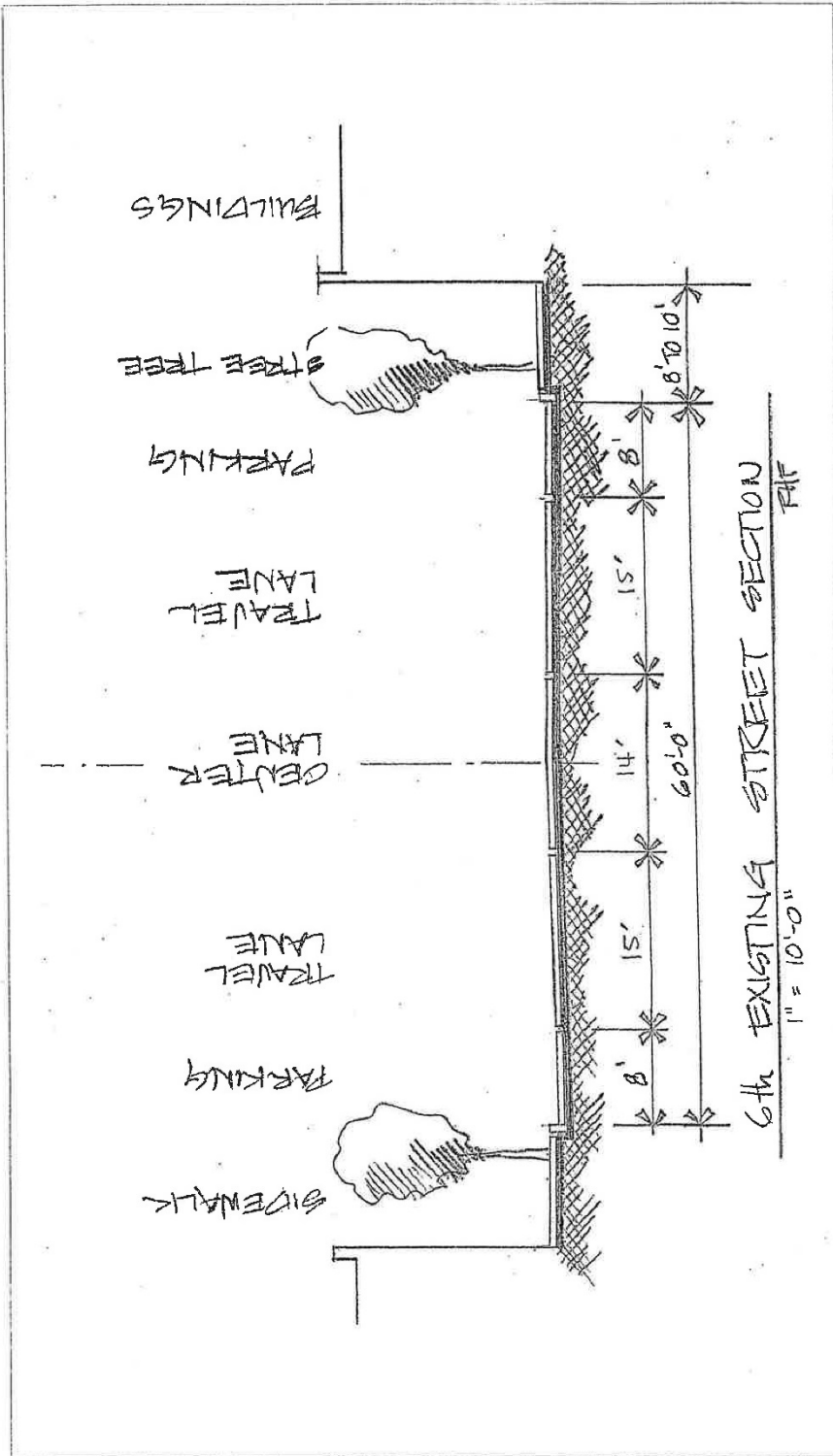


Figure 9.2-16

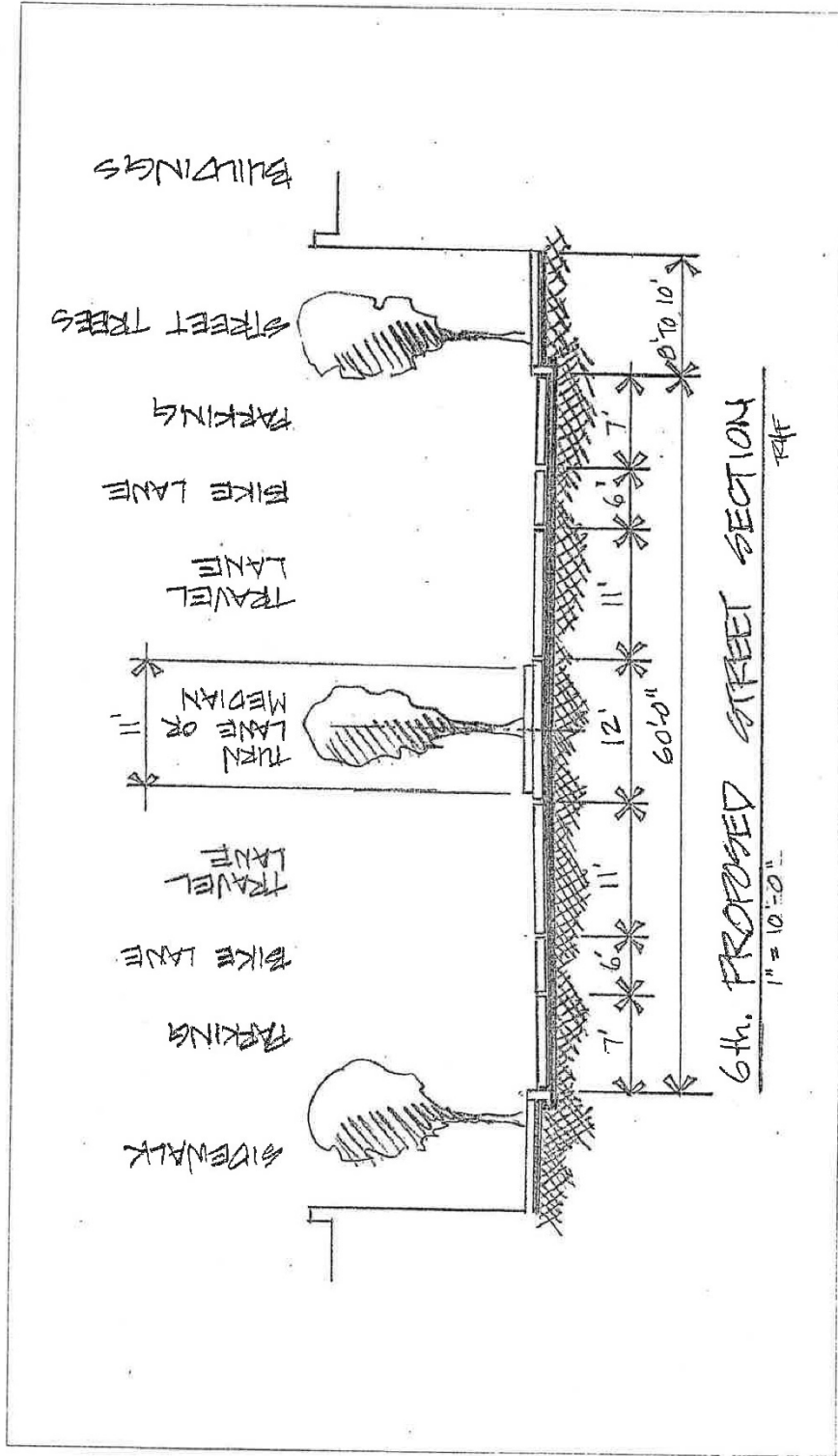
City of Umatilla Downtown
Revitalization and Circulation Study

Downtown Housing
Concept Using Mixed Use Page 83



Typical Existing 6th Street Cross Section
 Figure 9.2-17

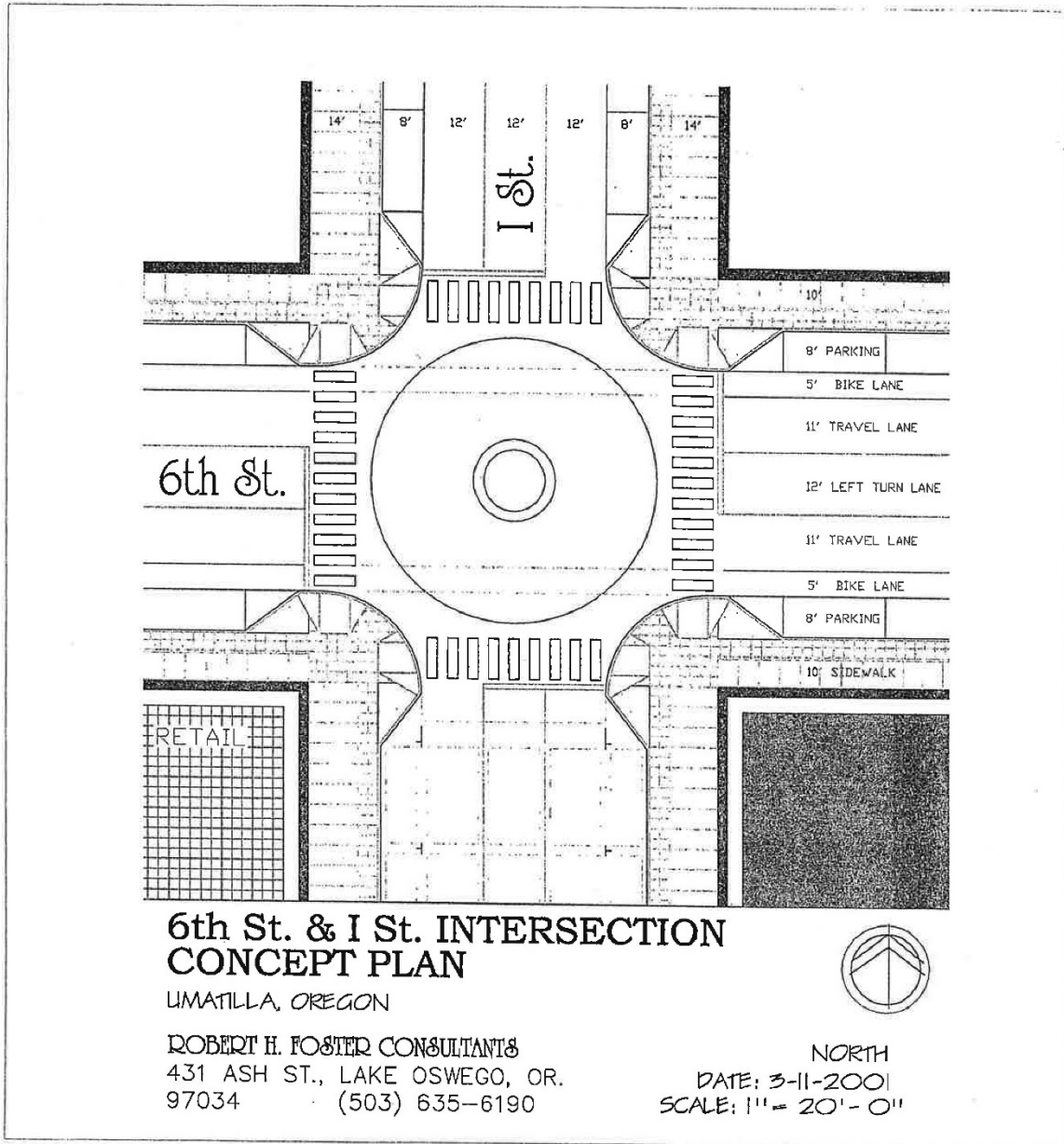
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Proposed 6th Street Cross Section
 Figure 9.2-18

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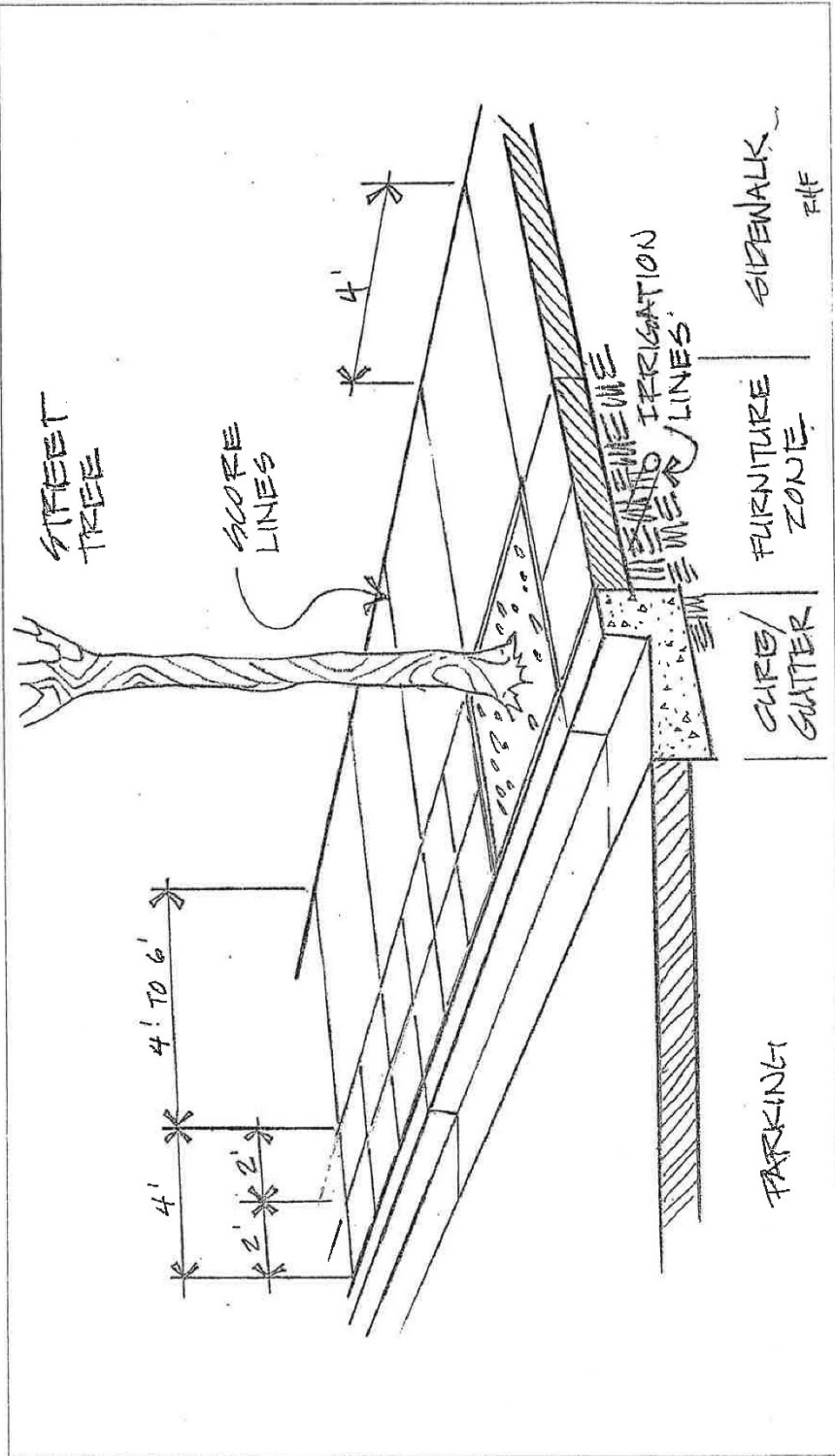
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6th Street and "I" Street
 Intersection Concept Plan
 Figure 9.2-19

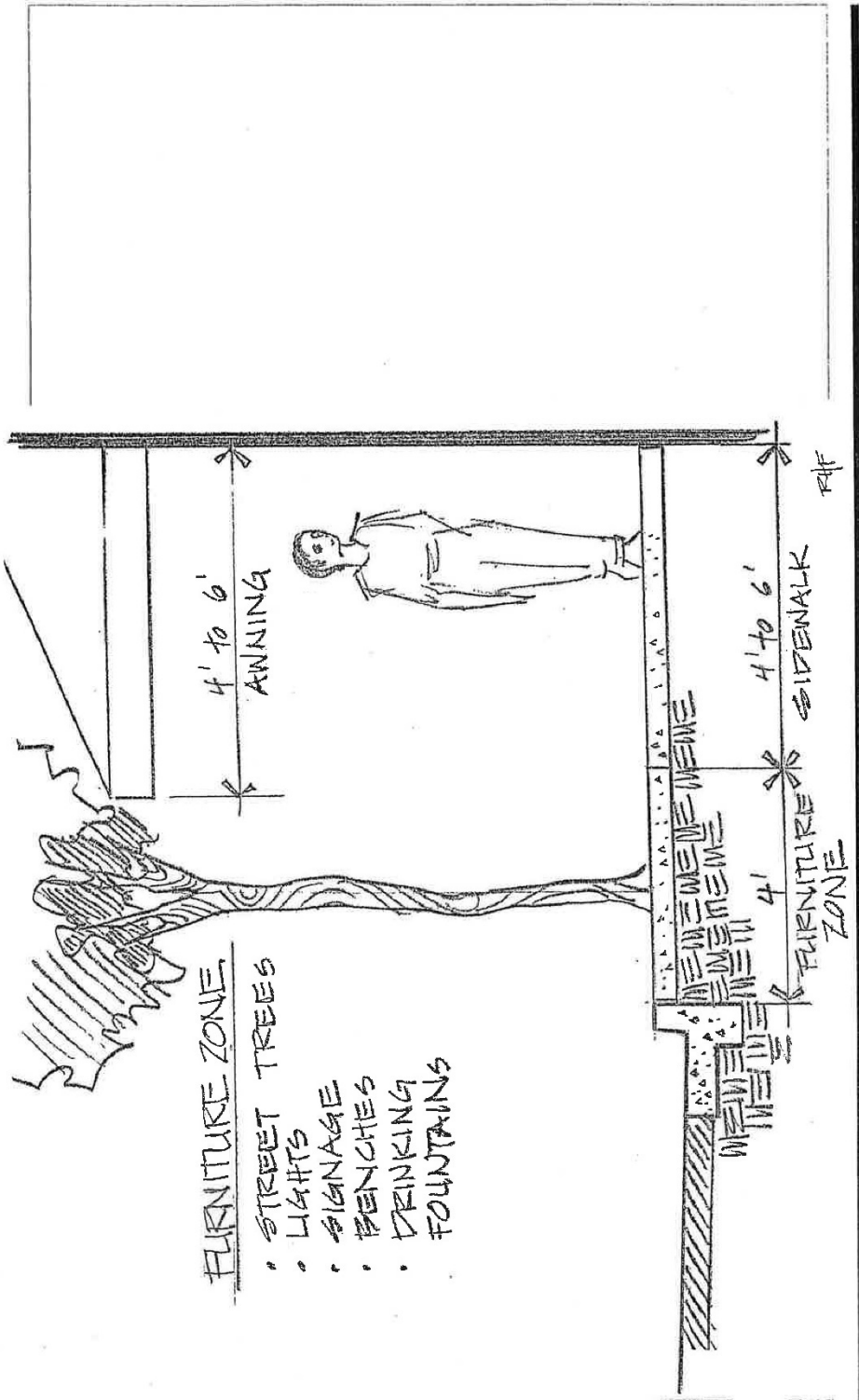
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 Page 86



Typical Sidewalk Pavement Section
 Figure 9.2-20

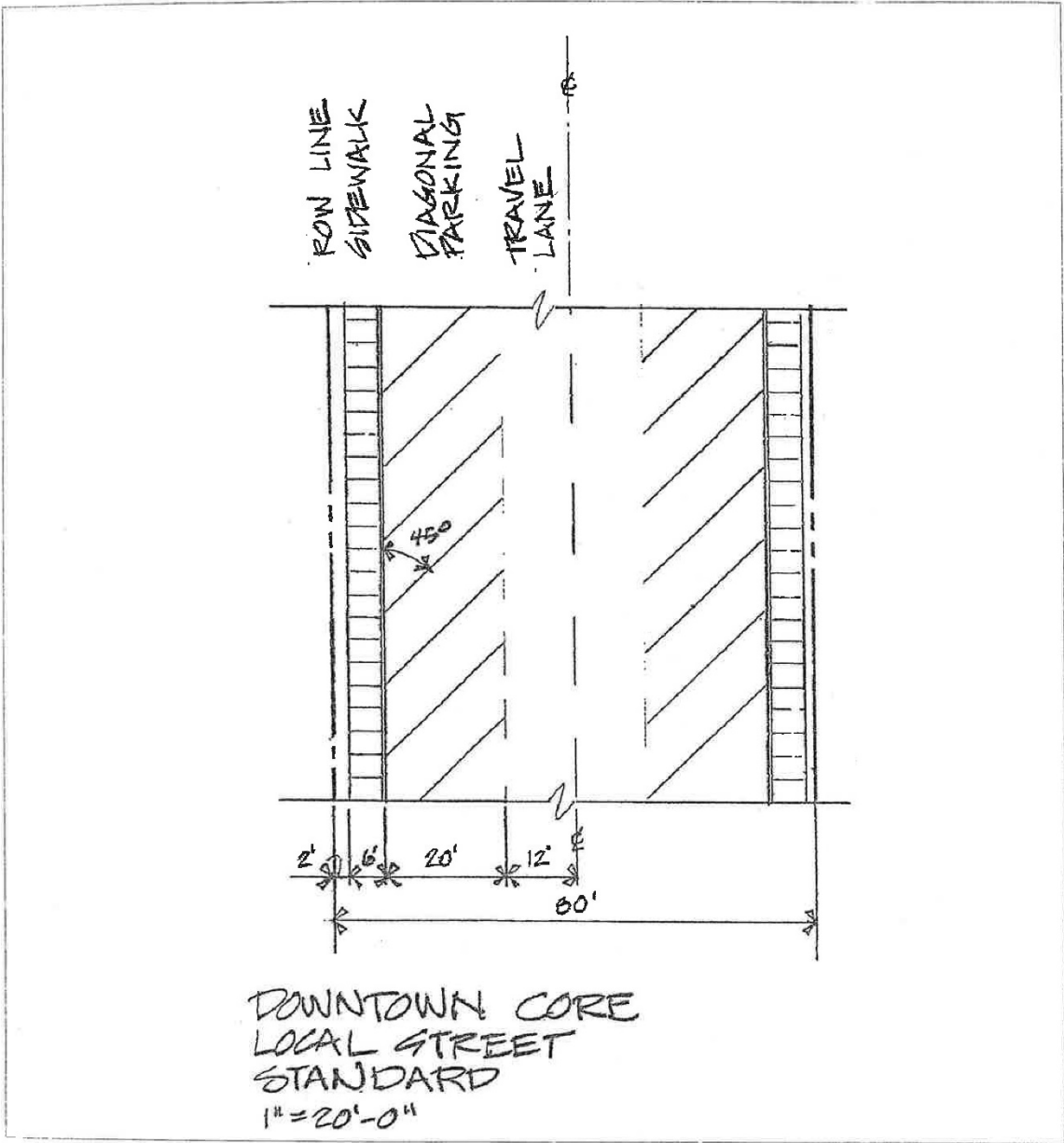
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Typical Furniture Zone on Sidewalk Section
 Figure 9.2-21

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Downtown Core Local Street Standard
Figure 9.2-22

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Unit costs used to prepare the estimates are summarized in Table 9.2-9. Many of the items listed area available in a range of different prices. For example, the sample prices for tree grates ranged from \$100 to \$275 per grate. Sample street lights prices ranged between \$1,700 to \$3,000 per light. Median prices are shown in the table below and are adjusted to include installation.

Table 9.2-9. Unit Costs of Improvement Project Items

| ITEM | UNIT | COST/UNIT |
|--------------------------|------|------------|
| Demolition | SF | \$1.00 |
| Curbs | LF | \$15.00 |
| Street Patching | SF | \$6.00 |
| Trenching | LF | \$15.00 |
| Sidewalk | SF | \$7.00 |
| Trees | EA | \$500.00 |
| Tree Grates | EA | \$190.00 |
| Irrigation (drip system) | LF | TBD |
| Landscaping | SF | \$5.00 |
| Planters (trough) | EA | \$600.00 |
| Street Lights | EA | \$2,250.00 |
| Trash Cans | EA | \$750.00 |
| Bench | EA | \$1,000.00 |
| Drinking Fountain | EA | \$3,000.00 |
| Bike Rack | EA | \$500.00 |
| Painted Crosswalk | SF | \$1.00 |
| Concrete Crosswalk | SF | \$8.00 |

Table 9.2-10 summarizes the improvement projects by phase and estimated total cost. The Streetscape and Pedestrian Improvements Phasing Plan in *Figure 9.2-23* illustrates how the projects might be implemented through a sequence of many projects.

Table 9.2-10. Streetscape Improvement Projects by Phase

| <u>Project Name/Phase</u> | <u>Estimated Total Cost</u> |
|---|-----------------------------|
| <i>Phase 1 Projects</i> | |
| <i>Intersection at 6th and I Streets with Curb Extensions</i> | <i>\$65,000</i> |
| <i>Intersection at 6th and H Streets with Curb Extensions</i> | <i>\$61,000</i> |
| <i>Intersection at 6th and G Streets with Curb Extensions</i> | <i>\$61,000</i> |
| <u><i>Subtotal</i></u> | <u><i>\$187,000</i></u> |
| <i>Phase 2 Projects</i> | |
| <i>6th Street Sidewalks between H and I Streets</i> | <u><i>\$106,000</i></u> |
| <i>6th Street Sidewalks between G and H Streets</i> | <u><i>\$106,000</i></u> |
| <i>6th Street Median between G and H Streets</i> | <u><i>\$30,000</i></u> |
| <u><i>Subtotal</i></u> | <u><i>\$242,000</i></u> |
| <i>Phase 3 Projects</i> | |
| <i>6th Street Median between I and J Streets</i> | <i>\$30,000</i> |
| <i>Intersection at 6th and J Streets with Curb Extensions</i> | <i>\$57,000</i> |
| <i>6th Street Sidewalks between I and J Streets</i> | <i>\$106,000</i> |
| <u><i>Subtotal</i></u> | <u><i>\$193,000</i></u> |
| <i>Phase 4 Projects</i> | |
| <i>I Street Sidewalks between 6th and 7th Streets</i> | <i>\$133,000</i> |
| <i>I Street Sidewalks between 5th and 6th Streets</i> | <i>\$133,000</i> |
| <i>South of Intersection at Corner of 5th and I Street</i> | <i>\$66,500</i> |
| <u><i>Subtotal</i></u> | <u><i>\$332,500</i></u> |
| <i>Phase 5 Projects</i> | |
| <i>6th Street Median between F and G Streets</i> | <i>\$30,000</i> |
| <i>Intersection at 6th and F Streets with Curb Extensions</i> | <i>\$61,000</i> |
| <i>6th Street Sidewalks between F and G Streets</i> | <i>\$106,000</i> |
| <i>6th Street Sidewalks between E and F Streets</i> | <i>\$106,000</i> |
| <u><i>Subtotal</i></u> | <u><i>\$303,000</i></u> |

Table 9.2-10. Streetscape Improvement Projects by Phase (Continued)

| | |
|--|--------------------|
| <i>Phase 6 Projects</i> | |
| <i>Gateway Feature (West End)</i> | \$42,000 |
| <i>Intersection at 6th and E Streets with Curb Extensions</i> | \$61,000 |
| <i>Intersection at 6th and D Streets with Curb Extensions</i> | \$57,000 |
| <u><i>Subtotal</i></u> | <u>\$160,000</u> |
| <i>Phase 7 Projects</i> | |
| <i>6th Street Sidewalks between D and E Streets</i> | \$106,000 |
| <i>6th Street Median between D and E Streets</i> | \$30,000 |
| <i>Intersection at 7th and I Street (Special Condition)</i> | \$57,000 |
| <i>Sidewalks south of 7th and I Street Intersection</i> | \$66,500 |
| <u><i>Subtotal</i></u> | <u>\$153,500</u> |
| <i>Phase 8 Projects</i> | |
| <i>Intersection of 6th and Switzler Avenue</i> | \$61,000 |
| <i>Intersection of 6th and Yerxa Avenue</i> | \$61,000 |
| <i>Intersection of 7th and Switzler Avenue</i> | \$34,000 |
| <i>6th Street Sidewalks east of Yerxa Avenue</i> | \$106,000 |
| <u><i>Subtotal</i></u> | <u>\$262,000</u> |
| <i>Phase 9 Projects</i> | |
| <i>Sidewalks north of 6th Street and Switzler Avenue</i> | \$53,000 |
| <i>Sidewalks east of Intersection of 6th Street and Yerxa Avenue</i> | \$106,000 |
| <u><i>Subtotal</i></u> | <u>\$159,000</u> |
| <i>Phase 10 Projects</i> | |
| <i>6th Street Sidewalks between Switzler and Yerxa Avenues</i> | \$176,000 |
| <i>6th Street Median between Switzler and Yerxa Avenues</i> | \$30,000 |
| <u><i>Subtotal</i></u> | <u>\$206,000</u> |
| <i>Phase 11 Projects</i> | |
| <i>Switzler Avenue Sidewalks between 6th and 7th Streets</i> | \$102,000 |
| <i>Gateway Feature (East End)</i> | \$42,000 |
| <u><i>Subtotal</i></u> | <u>\$144,000</u> |
| <u><i>Total Estimated Cost for all Phases</i></u> | <u>\$2,395,000</u> |



LEGEND
 Bulbouts
 Street Trees
 Median

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Figure 9.2-23 Improvement
 Phasing Plan

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9.2.423 *Improvement Descriptions*

9.2.423(1) *Switzler and Yerxa Avenues*

Two intersections on the east side of downtown Umatilla should be improved: the intersection of 6th Street and Switzler Avenue, and the intersection of 6th Street and Yerxa Avenue. The intersection of 6th Street and Switzler Avenue is important because Switzler Avenue provides a link to the neighborhoods to the north. It is also a link to bicycle trail on 3rd Avenue. *Figure 9.2-24* illustrates a park concept that could be constructed on the northwest corner of the 6th Street/Switzler Avenue intersection. The intersection of 6th Street and Yerxa Avenue is important because Yerxa Avenue offers a connection between the High School and commercial establishments on 6th Street. Improvements should include, sidewalk bulb outs, street trees, and striped pedestrian crossings.

9.2.423(2) *West City Entrance*

The City can define the entrance to the City by creating a monument, sculpture, or other type of landmark feature. The feature can be further enhanced using accent lighting, paving and landscape elements. The location of the entrance feature should be along 6th Street near the intersection of C Street. *Figure 9.2-25* illustrates a gateway concept that could be implemented on the west city entrance.

9.2.423(3) *East City Entrance*

The bridge over the Union Pacific Railroad creates an entrance on the east side of the City. Adding a landmark or symbol can enhance this entrance. The landmark or symbol might take the form of a sculpture, monument, or sign. The entrance can be accented with historic lights. *Figure 9.2-26* illustrates a gateway feature for the east city entrance.

9.2.423(4) *Intersection of 6th and I Street*

The plan envisions the corner of 6th and I Streets as the commercial center for the downtown area because 6th Street is the primary vehicular route through town, and I Street is considered to have the greatest potential as a pedestrian-oriented cross street. Improvements to this intersection would include accent pavement, benches, and possibly a raised intersection.

9.2.423(5) *Close A Street Intersection*

To increase safety at the Umatilla River Bridge it is recommended that the intersection of A Street and 6th Street be closed by creating a cul-de-sac or dead end at the south end of A Street.

9.2.423(6) *Heritage Trail/Senior Center Parking*

A new parking area is proposed to serve both users of the Lewis and Clark Heritage Trail and the Senior Center. The joint parking lot is to be located at the intersection of B and 7th Streets.

Figure 9.2-24 Park Concept at the Northwest Corner of 6th Street/Switzler Avenue

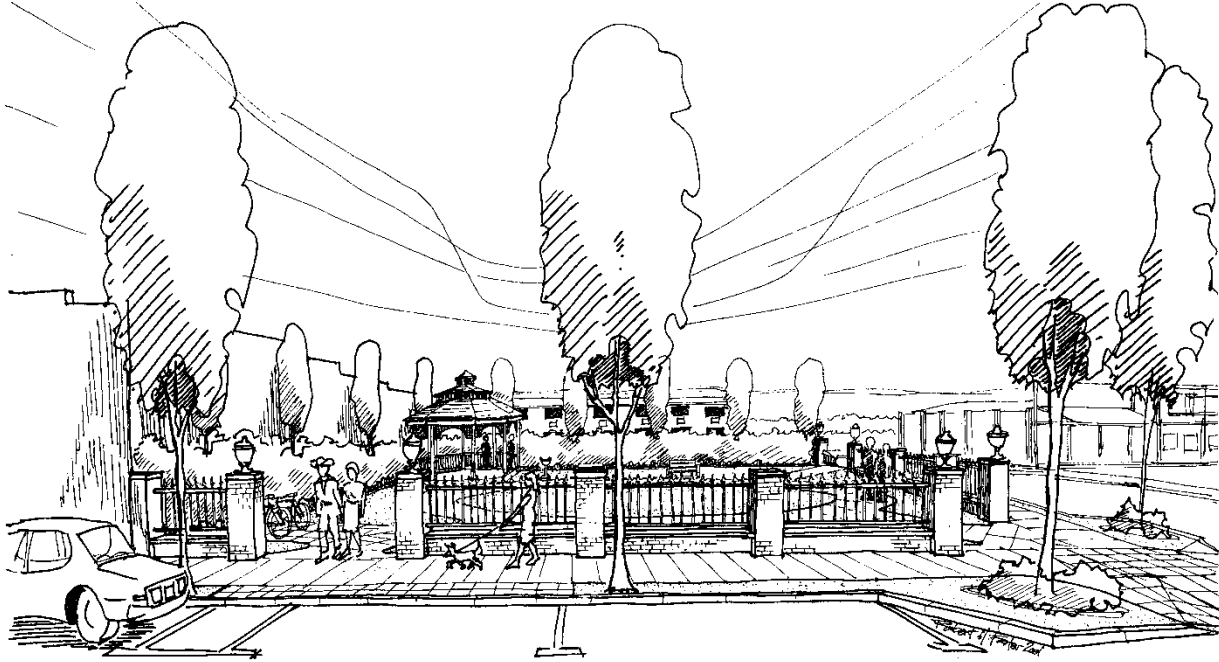
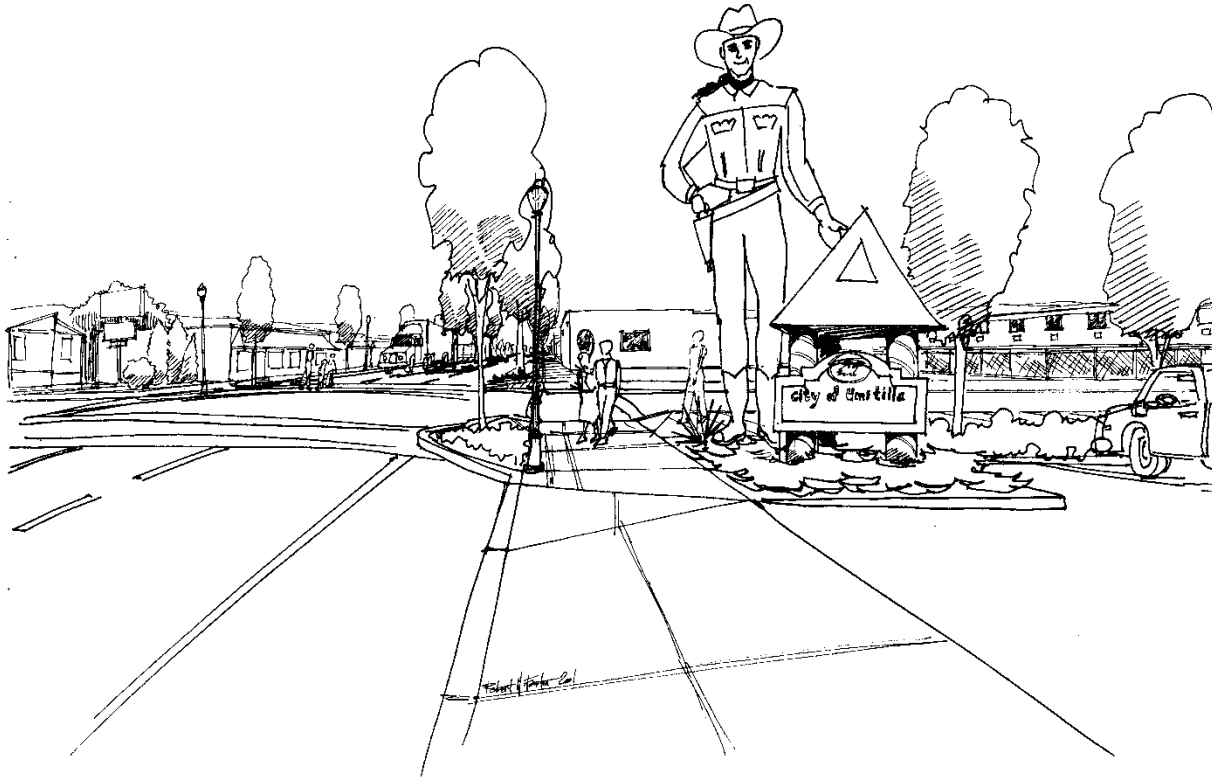


Figure 9.2-25 West City Entrance Gateway Feature



Figure 9.2-26 East City Entrance Gateway Feature



9.2.500 *CIRCULATION PLAN*

The circulation plan for the downtown study area is comprised of the following sections: functional classification, parking, street improvements, and access management. The supporting information used to create this information can be found in the Appendix. The majority of the supporting information can be found in the Opportunities and Constraints Report.

9.2.510 *Functional Classification*

The functional classification for the study area was taken from the 1999 City of Umatilla Transportation System Plan (TSP). In the study area, 6th Street (Highway 730) is the only major arterial. Five collectors exist in the study area. They are 7th Street from “B” Street to Umatilla River Road, “B” Street from 6th Street to 7th Street, “F” Street from 3rd Street to 7th Street, Switzler Avenue from 3rd Street to 7th Street, and 3rd Street. The remaining study area streets are local streets.

No streets are being proposed to be reclassified by this study. However, consideration should be given to create a pedestrian-oriented environment around “I” Street and portions of 6th Street within the downtown core area. This can be done by considering this area as a Special Transportation Area (STA).

9.2.511 *Parking*

The parking survey summarized in the Opportunities and Constraints report has documented that sufficient on-street parking exists on 6th Street to support the existing commercial uses.

Of the 160 on-street parking spaces along 6th Street between A Street and Jane Avenue, the peak usage occurred at 3:00 PM with only 23 spaces used. This is a maximum overall utilization of 14 percent. At the maximum overall parking utilization, 137 on-street parking spaces were available.

Based on the parking survey conducted, there seems to be more than sufficient parking supply for the immediate future. Should the City of Umatilla encounter parking problems in the distant future, a two tiered strategy could be implemented to meet any future demand. First, on-street parking spaces are not marked on the side streets. Additional spaces could be sought by striping parking spaces on the side streets. Conservatively, at least 100 spaces could be added from side street parking. Considerably more side street parking spaces could be developed if the local street cross-section depicted in *Figure 9.2-22* was implemented. Should the City of Umatilla add additional side street parking spaces, then it should consider installing curb, gutter, and storm drains at that time. The second strategy to add parking in the downtown area is to close one or two side streets between 6th and 7th Streets and develop surface street public parking lots. The closure of one or two side streets should not have a significant impact to traffic since the downtown area has a well-developed grid system. There is a side street approximately every 200 hundred feet. The only consideration are to avoid closure of the collector side streets which include “B” Street, “F” Street, and Switzler Avenue and “I” Street which is the center of the downtown revitalization concept. Each side street block has a right-of-way dimension of 80 feet by 200 feet and could accommodate at least 30 new parking stalls.

9.2.512 *TSP Street Improvements*

There are no roadway improvement projects planned in Umatilla's Transportation System Plan (TSP) for the study area. However, based on discussions with ODOT, the Umatilla River Bridge may be rebuilt in the next five or six years. Should this occur, the City of Umatilla would like to consider a new alignment for the bridge. The new alignment would cross the Umatilla River at 5th Street. Highway 730 would be re-aligned with 6th Street between "C" Street and "E" Street to the east. To the west, Highway 730 would be re-aligned to the current highway alignment west of Powerline Road. Powerline Road could then be re-aligned to the new highway alignment further west of the new bridge.

There are three traffic signal projects planned for the study area at the eastern and western boundaries of the study area. The three intersections identified in the TSP as needing signalization by 2017 are the Powerline Road/Highway 730, Umatilla River Road/Highway 730 (6th Street), and "J" Street/Highway 730 (fire signal) intersections.

Other than the streetscape plan and pedestrian improvements, no new street improvement project is proposed.

9.2.513 *Median Improvements Created by the Downtown Revitalization Plan*

In order to enhance the pedestrian friendliness, attractiveness and traffic calming along 6th Street, medians and bulb-outs are proposed. Medians narrow the area of pavement and may provide a refuge area for pedestrians. Bulb-outs create a perception of narrowing the street to the motorist. They are also used to make pedestrian crossings shorter, and therefore easier and safer. Both medians and bulb-outs narrow the area of pavement and travel lane widths providing a visual cue to the driver to slow down. When used in conjunction they can have a significant impact on travel speeds along the roadway. *Figure 9.2-7* shows the proposed Pedestrian and Streetscape Improvements, which illustrate the proposed locations of medians and bulb-outs.

The proposed medians will eliminate left turn pockets along 6th Street the following locations:

- Westbound left at D Street
- Eastbound left at E Street
- Westbound left at F Street
- Eastbound left at G Street
- Westbound left at G Street
- Eastbound left at H Street
- Westbound left at I Street
- Eastbound left at J Street

Although the proposed medians may replace the left turn pocket in several locations along 6th Street, left turn movements would still be allowed. No turn restrictions are being proposed.

Even though a median is proposed between Switzler Avenue and Yerxa Avenue, left turn pockets along 6th Street have been retained to provide access to the local businesses including the Red

Apple grocery store. Bulb-outs along 6th Street are proposed at all intersections where medians are proposed.

Replacing the left turn pockets with the proposed medians is not expected to have a significant impact on left turns from 6th Street to side streets for several reasons. There are side streets approximately every 200 feet. This provides numerous opportunities to make left turns, thereby spreading the left turn volumes on several intersections. Also, left turn volumes do not constitute a large percentage of traffic along 6th Street between D Street and J Street (there are less than 5 percent of the traffic at 6th Street/Switzler Avenue).

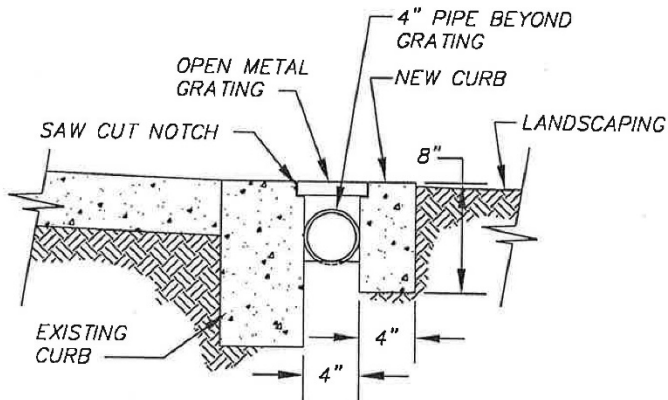
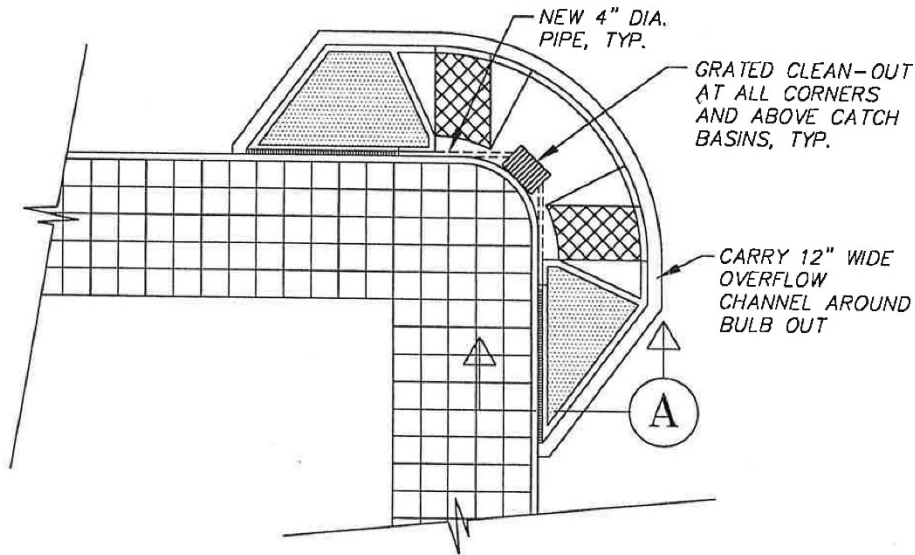
The redevelopment along 7th Street between H Street and I Street is expected to close 7th Street in this section permanently. Since 7th Street will no longer provide a continuous alternative to 6th Street through Umatilla, little or no diversion is expected in through traffic from 6th Street to local streets.

It is estimated that the reductions in travel speeds on 6th Street will also increase delay on the side streets by approximately five (5) to ten (10) percent. In addition, round-the-block circulation should not be impeded by the proposed medians and bulb-outs.

9.2.514 *Bulb-Out Design Detail*

The intent of the proposed bulb-out detail is to provide a compact layout for pedestrian access beyond the existing curbs in the downtown area. Seventy-five percent of the corners have no catch basins, and runoff is primarily directed to the north and west following the slope of the existing ground and adjacent highway grade. A small tapered landscaped area would be provided on either side of handicap ramps, with these ramps faced in two directions, set out from the present curb line. The bulb-out design detail described above is shown in *Figure 9.2-27*.

Drainage would pass underneath this bulb-out following the current flow line of the existing curb, utilizing a combination of an open top metal grated trough behind the landscaped area to provide for ease of cleanout, connections between this trough and around the corner with a grated hatch into a 4-inch pipe, with the additional construction of a 12-inch wide concrete apron around the bulb-out to carry overflows in the event of plugging of the drainage trough or 4-inch diameter pipe.



Bulb Out Design Standard Detail
 Figure 9.2-27

9.2.515 *Access Management*

Appendix 9A-4 has a detailed discussion of Umatilla's access management practice. The City's TSP documents the access management standards. The only proposed change to those standards by this plan is to update the ODOT facility standards to meet the standards set in the recently adopted 1999 Oregon Highway Plan (OHP). Also, the Downtown Core and Downtown Transitional areas should be considered as a Special Transportation Area (STA).

9.2.520 *Special Transportation Area Designation in Downtown Umatilla*

Access management is important to promoting safe and efficient travel for both local and long distance users along State Highway 730 in the City of Umatilla. The 1999 *Oregon Highway Plan* specifies an access management spacing standards and policies for state facilities. Although the City of Umatilla may designate state highways as arterial roadways within their transportation system, access management for these facilities follows the Access Management Spacing Standards of the 1999 Oregon Highway Plan. These spacing standards are based on highway classification, type of area and speed, which are shown in the appendix to this document. This section describes the state highway access management objectives and specific highway segment where special access spacing standards apply.

Highway 730 through the City of Umatilla is categorized in the 1999 Oregon Highway Plan as a Regional Highway. The primary function of these highways is to provide connections and links to regional centers, Statewide or Interstate Highways, or economic or activity centers of regional significance. The management objective of Regional Highways is to provide safe and efficient, high speed, continuous-flow operation in rural areas and moderate to high-speed operations in urban and urbanizing areas. A secondary function is to serve land uses in the vicinity of these highways. Inside STAs (Special Transportation Areas), local access is also a priority. Inside Urban Business Areas, mobility is balanced with local access.

To assist in implementing state access management standards and policies, the 1999 Oregon Highway Plan also recognizes that state highways serve as main streets of many communities, such as downtown Umatilla. Shorter block lengths and a well-developed grid system are important to a downtown area, along with convenient and safe pedestrian facilities. In general, downtown commercial arterial streets typically have blocks 200 to 400 feet long, driveway access sometimes as close as 100-foot intervals and occasionally, crosswalks, along with on street parking. The need to maintain these typical downtown characteristics must be carefully considered along with the need to maintain the safe and efficient movement of through traffic. The Oregon Highway Plan recognizes the main street function through the designation of Special Transportation Areas (STAs).

9.2.521 *Special Transportation Area*

A Special Transportation Area (STA) is a designation that may be applied to a state highway, when a downtown, business district or community center straddles the state highway within a community's urban growth boundary. STAs can include central business districts but they do not apply to whole cities or strip development areas along individual highway corridors.

The primary objective of a STA is to provide access to community activities, businesses and residences, and to accommodate pedestrian, and bicycle movements along and across the highway

in a compact central business district. An STA designation will allow reduced mobility standards, accommodate existing public street spacing and compact development patterns, and enhance opportunities to provide improvements for pedestrians and bicyclists in the downtown area. Inclusion in an STA allows for redevelopment with exception to the proposed access management standards.

Access management in STAs corresponds to the existing city block for public road connections and discourages private driveways. However, where driveways are allowed and land use patterns permit, the minimum spacing for driveways is 175 feet or mid-block if the current city block spacing is less than 350 feet. In addition, the need for local street connections may outweigh the consideration of maintaining highway mobility within an STA.

In Umatilla, the area along Highway 730 between “A” Street (milepost (M.P.) 182.66) and River Road (M.P. 183.52) exemplifies the design features that would be consistent of an urban downtown area designated as an STA. Within this mile segment, buildings are spaced close together, parking is on street, and the posted speed limit is 25 m.p.h. The compact development pattern qualifies this area for an STA highway segment designation.

After adoption of the TSP by the Umatilla City Council and a finding of compliance with the Oregon Highway Plan, the City of Umatilla and ODOT Region 5 may jointly designate this segment of Highway 730 as an STA through a Memorandum of Understanding (MOU). The MOU should incorporate by reference the TSP and the following STA Management Plan provisions.

9.2.522 *Special Transportation Area Management Plan*

The Umatilla STA is located on the portion of Highway 730 (6th Street) between the intersections of “A” Street (M.P. 182.66) and River Road (M.P. 183.52), which is located completely within the urban growth boundary and city limits of the City of Umatilla.

The primary objective of the Umatilla STA is to provide access to community activities, businesses and residences, and to accommodate pedestrian, and bicycle movements along and across the highway in the city’s central business district.

The designation of an STA in Umatilla is intended to accommodate the existing public street spacing and compact development pattern. Specific access management conditions for the Umatilla STA on Highway 730 include:

- a) Minimum spacing for public road connections at the current city block spacing of 200 feet.
- b) Public road connections are preferred over private driveways. Private driveways are discouraged in an STA.
- c) Where land use patterns permit, ODOT will work with the City and property owners to identify appropriate access to adjacent property owners within the STA.
- d) Where a right to access exists, access will be allowed to property at less than the designated spacing standard only if the property does not have reasonable alternative. If possible, other options should be considered, such as joint access.

- e) Where a right to access exists, the number of driveways to a single property shall be limited to one. ODOT will work with the City and property owners if additional driveways are necessary to accommodate and service the traffic to the property, and will not interfere with driver expectancy and the safety of through traffic on the highway.
- f) Driveways shall be located where they do not create undue interference or hazard to the free movement of normal highway or pedestrian traffic. Locations in areas of restricted sight distance or at points that interfere with the placement and proper functioning of traffic control signs, lighting or other devices that affect traffic operation will not be permitted.
- g) If a property is landlocked (no reasonable alternative exists) because a driveway cannot be safely constructed and operated and all other alternatives have been explored and rejected, ODOT might be required to purchase the property. However, if a hardship is self-inflicted, such as by partitioning or subdividing a property, ODOT has no responsibility for purchasing the property.

Traffic on the state highway operates at LOS C or better, which correlates to maximum volume to capacity ratio of 0.69. Increase in traffic volumes over the 20-year projection period is expected to increase significantly based on Umatilla's Transportation System Plan in Chapter 12. The level of service and correlating volume to capacity ratio is projected at LOS E and F which correlates to a volume to capacity ratio greater than 0.80 which is the maximum standard for Highway 730 within the city's urban growth boundary. This projection from the TSP is based on an aggressive growth assumption, which may not occur. The volume to capacity ratio of Highway 730 through the STA area of Umatilla should be monitored periodically to assure that the state highway is operating at or better than the established standard.

To maintain highway mobility through an STA in Umatilla, land use development decisions (within the urban growth boundary) shall not cause traffic flow to exceed a volume to capacity ratio of 0.80. The posted speed limit in the STA is currently and will remain at 25 miles per hour as allowed by state statute in a business district. Curb (parallel or perpendicular) parking is permitted in the STA, provided minimum sight distance requirements are met for all public road connections and private driveways. Parking in this area is adequate at this time. No signals or traffic control devices currently exist in this area.

To enhance Highway 730 for pedestrians and bicycles, there is a proposal to change the use of the existing street cross-section. Highway 730 is currently a three-lane roadway with parallel parking on both sides of the street. Sidewalks generally exist along the highway but no bike lanes exist. The proposal is to restripe Highway 730 to include one travel lane in each direction, maintain the on-street parking, and stripe bicycle lanes on both sides of the highway. In addition, medians are planned in place of the center turn lane in certain sections. The proposed cross-section was shown previously in *Figure 9.2-18*.

The designation of an STA in Umatilla further identifies the need to accommodate pedestrian, and bicycle movements along and across the highway in the central business district. The recommended urban arterial standard within the STA consists of an 80-foot right-of-way with a paved width of 60 feet that includes two 11-foot travel lanes, 6-foot bike lanes, 7-foot parking strip on each side of the road, and a center median of 12 feet. The standard includes 8 to 10 foot

sidewalks on each side of the road that includes a four-foot furniture zone that can accommodate street lighting and street trees. To accommodate bicycle movements along the highway, bike lanes should be installed along the entire length of the STA boundary. Bulb-outs have also been proposed throughout most of the STA area. *Figure 9.2-23*, shows the locations where bulb-outs are being proposed. There are no other bicycle and pedestrian improvements identified in this area.

Another essential component to accommodate pedestrians in an STA is street crossings. There is one crosswalk enhancement proposed. *Figure 9.2-19* shows the proposed crosswalk improvements to the intersection of Highway 730 (6th Street) and “I” Street. There are no other specific crosswalk enhancements or safety improvements recommended within the STA at this time other than the bulb-outs previously mentioned. Future improvements and modifications to the highway within the STA and within the curb line, or if no regular established curb, to the right-of-way utilized for highway purposes will be made in accordance with the Oregon Highway Design Manual and with ODOT approval.

Existing maintenance and operational strategies along Highway 730 will be employed within the STA, consistent with Oregon Revised Statute 373.020, as follows:

- ODOT shall be responsible for the ongoing maintenance of: a) the roadway surface between curbs, or if no regular established curb, to that portion of right-of-way utilized for highway purposes b) painting centerline stripe, c) designated school crosswalk delineation, directional and regulatory signs except those signs described as the City’s responsibility and d) plowing snow one blade-width of centerline stripe provided there are no conflicts with utilities.
- City shall be responsible for the on-going maintenance of: a) storm sewer system, b) sidewalks, c) landscaping, d) luminaries, e) U-turn signs, parking signs, and street name signs, f) painting parking-stripes and other pavement delineation not described as ODOT’s responsibility, and g) snow removal from parking strip.

Future improvements and modifications to the highway within the STA will include maintenance and operational strategies with ODOT and City approval.

9.2.600 *IMPLEMENTATION*

Implementation concepts can be broadly categorized as either downtown redevelopment strategies or changes to policies and plans. Redevelopment strategies will enable the City to create an economic development program to make it more attractive to commercial and industrial businesses. The strategies also identify funding sources to assist with implementation of public improvement projects defined in the Plan. Ideally, changes in regulatory policies should remove any regulatory obstacles to development within downtown Umatilla and guide prospective development to achieve the community's goals. This chapter begins with a discussion of downtown redevelopment strategies and then identifies recommended changes in regulatory policies such as the Comprehensive Plan and Zoning Ordinance.

9.2.610 *Downtown Redevelopment Strategies*

The civic center concept and other conceptual design work done to date for this project will be helpful in the creation of a more compact downtown and a sense of place. However, it appears the City of Umatilla also needs to address commercial development and business recruitment and retention at a more basic level. This section primarily speaks to those issues, but also identifies potential funding sources for both proposed improvements to the downtown and commercial development.

While undergoing efforts to improve the physical appearance of the City and encourage commercial development it will be important to keep in mind that getting the first few new businesses in will be the most difficult. Also, it will most likely be easier to get new businesses in than to get existing businesses to relocate.

Development of tools for marketing and business recruitment will be a necessary first step.

9.2.611 *Getting Ready for Economic Development/Marketing*

Most economic development and marketing activities begin with compilation and interpretation of background data. Getting the work done can be challenging because of the time commitment required and what often seems a lack of immediate results. However, long-term success at improving the economic vitality of Umatilla may hinge on getting this up-front homework done. Key development/marketing tools could be expected to include:

1. *Business and property inventories.* Such an inventory would include information on land and building sizes, current use, and property ownership. A database can be used to store this information. Periodic updating of the business and property database is recommended as information about individual properties change, or at least once a year.
2. *Contact and survey Umatilla business and property owners.* Surveying these groups both provides key information and a critical opportunity to get their buy-in on the City's efforts. Completed questionnaires should be entered into a computer database. A brief report summarizing results should be prepared and made available to City staff as well as participants. Care should be taken to assure 1) that questions are limited in number and will provide usable results, and 2) results are aggregated in the report in such a way that confidential information is not attributed to any particular business or property owner.

3. *Coordinate customer survey(s)*. Two optional approaches may be taken to surveying current and/or prospective customer bases. The first technique involves use of *intercept* surveys — targeted to those who already come to area businesses. Examples of ways to conduct intercept surveys include: 1) distribution of survey cards by business owners to customers passing through the premises (with drop boxes available for completed survey cards); or 2) posting volunteers who ask survey questions at cooperating businesses and/or strategic places such as key street corners.

A second optional approach involves gathering a *random sample* of the population of Umatilla trade area and (potentially) beyond. The most common way to conduct this type of survey is through a telephone survey, with as many completed responses as possible obtained for statistical validity.

4. *Organizing economic and demographic data in a form useful for marketing Umatilla*. The final piece of the data collection effort involves organizing key information into a marketing piece useful to the City's marketing efforts. The purpose is to provide statistical indicators of business potential to market to possible investors. Demographic and economic indicators typically covered include:

- Population — and change over time.
- Age of population — compared with county or city-wide averages.
- Number of households — and household size.
- Household incomes — and change over time.
- Business cluster analysis — to define the mix that is uniquely Umatilla.
- Umatilla employment — and change over time.
- Sales leakage estimates — both for retail and services.

Focus should be placed on the positive trends; for instance increasing incomes, households with family needs, product orientation to take advantage of growing Hispanic population. In addition, updates to this information should be made as needed; especially when there are significant new data sources available (such as the year 2000 Census).

9.2.612 *Marketing Approach & Products*

What should be done with all this market information? Several specific products and activities are recommended. For each step recommended here, it will be necessary to identify responsible parties.

1. *Assemble a business development/marketing package*. Whether the target is the owner of an existing business or a prospective investor or developer, the best results are obtained by *customizing* information to the needs and interests of the recipient. Economic development organizations often find that a well-designed folder with pockets for a variety of materials provides the greatest flexibility together with ease of presentation.

Examples of materials that could be readily compiled follow. These could be available on computer and provided as needed. Each particular insert might be anywhere from 1-4 pages long:

- Summary results of customer, business and property owner surveys -- with particular focus on identified business gaps/opportunities.
 - Business inventory/directory and map.
 - Target business list — with reasons why Umatilla represents a good location choice for each business type targeted.
 - Success stories — highlighting examples of businesses that have done well in Umatilla and why (ideally with photos and quotes from the owners).
 - Listing of available properties together with rental rates and/or sales prices -- prepared in collaboration with property owners, management companies or realtors as appropriate.
 - Historical information and general description of the City of Umatilla -- including incentive programs and other financial assistance available.
 - Summary of community demographic and economic trends.
2. *Prepare a business directory.* The directory should include a map of the district showing business locations, cross-referenced by type. If possible, the directory should be updated annually. A completed business directory can serve multiple audiences — including visitors, regular customers and employees, though its intended use may be primarily for insertion in business development/recruitment folders.
 3. *Organize a business recruitment program.* Once the data tools and marketing package are in place, it is time to put all of this information to good use. The most effective recruitment program begins locally and expands outward. First, identify current district business and property owners who may have the resources or know-how to open targeted business. After local contacts have been made, identify and contact successful businesses operating elsewhere in the county that might represent a *good fit* for a Umatilla location. Final areas of contact may include selected comparable businesses elsewhere in the Northeast Oregon/ Southwest Washington region.

For maximum effectiveness, a *tag team* approach is suggested for contacting desired businesses. This approach involves a recruitment coordinator and an individual who has an existing or potential peer relationship with the type of business being solicited (such as members of a local chamber of commerce or merchants association or even an ad hoc committee). A database of business contacts should be maintained with a tickler file for follow-up as warranted.

4. *Define and activate business finance and incentive programs.* Business finance programs may include anything from a façade improvement program to the possibility of setting up a shared risk pool focused on business lines of credit for existing or start-up businesses.

For instance the City of Pendleton created an incentive and marketing program for a four-block area in the downtown. The City, Pacific Power, RCDC, but primarily a grant from Oregon Economic and Community Development Department funded this program. It is a one-time program that continues until the \$45,000 is spent. The incentive program is structured so that if a business moves into the four-block area it receives \$1.00 for every square foot (SF) leased,

\$0.25 per SF for advertising, and \$200 for educational purposes. The goal is to cut vacancies in the four-block area in half. Funds are also going toward the downtown's share of freeway billboard cost and promotions of main street area. Reportedly the incentive program is probably not bringing in businesses to downtown Pendleton but businesses coming in are getting off to a stronger start.

5. *Plan and execute Umatilla promotions and events.* Specific business district-related events and promotions could play an important role, especially as the commercial district becomes more active. Potential events may be focused toward the general public or customers, or aimed at business recruitment targets. Results of the surveys and demographic and economic analysis may suggest opportunities for events that address Umatilla's best market prospects (for instance a Cinco de Mayo event).
6. *Jointly promote and advertise.* Promotional efforts need not be limited to events. Better information on business/property owner and customer interests should make it possible to fashion cooperative business promotion and advertising campaigns. Whatever the opportunity, the decision of where to spend marketing resources will be more informed and have a greater chance for success if based on solid marketing data. Celebrate the City's racial and ethnic diversity, the history of Umatilla, and the City's roles as the location where the Columbia River reaches Oregon and as a gateway to the state.
7. *Cultivate strategic partners.* Let potential financial partners know about efforts being made, including results of studies and surveys. One of the goals should be to increase awareness of the City of Umatilla and the potential role that the City can play in the overall economic development of county and region. Cultivating this interest is important, because it can evolve into financial partnerships required for complex public/private development.

In addition to the aforementioned marketing approaches, it is important to make the process for starting a business in Umatilla as easy and pleasant as possible. Providing clear zoning codes/ordinances without too much room for administrative discretion, including specific use lists with a limited number of uses relegated to conditional use. In order to stimulate activity, the City may want to be liberal in that most uses are allowed somewhere. For instance, auto related uses may not be allowed in the downtown core but should be allowed at interchange. The focus should be to make it simple and straightforward to locate in Umatilla.

9.2.620 *Potential Funding Sources*

The following two tables list potential funding sources for strategic investment in downtown Umatilla. The first table includes the most commonly used or high-priority funding resources and the next table includes other or secondary resources.

Table 9.2-11. Potential Funding Sources for City of Umatilla Strategic Investment

| Commonly Used/High Priority Funding Sources | |
|--|--|
| Funding Resource | Description |
| City of Umatilla | City funds for downtown projects can come from the general fund or a dedicated fund. Examples of dedicated funds include transient room tax, meal tax, etc. Detailed descriptions of some of dedicated funding resources follow. |
| Local Improvement District (LID) | LIDs are used to finance public improvements and facilities by distributing the cost of the improvements to surrounding property owners. Costs are typically distributed to property owners by street footage or land area, but can also be allocated based on size or assessed value of property or building. LIDs allow property owners to finance their assessment over time at the cost of public borrowing. |
| Economic Improvement District (EID) | EIDs are an alternative to type of assessment district exclusive to commercial and industrial areas. EIDs can be used to fund intangible improvements such as planning, management, promotion or maintenance in addition to development or improvement activities. Assessments cannot exceed 1% of real market value of property located within the district in any given year. |
| Revolving Loan Funds | The City can develop and provide revolving loan funds for infill projects. Specific eligibility requirements can be attached to these funds. This is especially useful where attention to detail is required. |
| Tax Increment Financing (TIF) | Tax increment financing provides dedicated public funds for planned physical improvements in a specific geographic area. This process requires designation of a “blighted”* Urban Renewal Area, creation of an Urban Renewal Agency and development of Urban Renewal Plan. Tax increment funds accumulate through increases in property values in the renewal district. Recent small town strategies have included structuring the URA to include both downtown commercial areas and those along highways. In this way development along the highway also functions as a source of tax increment funding that can be utilized for redevelopment of the downtown. |
| Business License Fee | Business license fees are placed on businesses within certain boundaries (for instance within an EID). The business license fee may be in the form of a surcharge on an existing license fee. The City is responsible for collection of fees and disbursement of funds. |
| Transient Room Tax | Additional funding could also potentially come from reallocating a percentage of the transient room tax (TRT) for debt service on revenue bonds or by increasing the tax rate. |
| Community Development Block Grants (CDBG) | Funds are available on a competitive basis for public improvements that benefit low/moderate income households. Some cities have received funding for public improvement projects or loans for private businesses for economic development purposes. |
| Oregon Housing and Community Services (OHCS) Community Incentive Fund | This fund is one part of the “Oregon Livability Initiative” and provides financing for development projects which help communities meet one or more of the following objectives: 1) Create more jobs in distressed ² communities that want economic growth; 2) increase the supply of affordable housing near jobs and transportation; 3) Reduce sprawling development patterns; 4) Revitalize urban centers, downtowns, and main streets. Awards have been determined for the current biennium. The earliest new applications would be accepted is fall of this year subject to legislative refunding of the program. A well-defined project scope and budget and committed financial resources on the part of submitters is necessary to compete effectively for these funds. |

² The economic distress index as employed by Oregon Economic and Community Development is used.

Table 9.2-11. Potential Funding Sources for City of Umatilla Strategic Investment - (continued)

| Commonly Used/High Priority Funding Sources | |
|---|--|
| Funding Resource | Description |
| Oregon Department of Transportation (ODOT), Transportation Enhancement Program | Oregon Department of Transportation (ODOT) has financial assistance programs, such as the Transportation Enhancement Program, to help implement projects such as the pedestrian and streetscape improvements. This program allocates federal TEA-21 funding for projects to enhance the cultural, esthetic, and environmental value of the state's transportation system. Federal funding is available to a maximum of 89.73%. Matching funds are a minimum of 10.27% of the project cost. |

Table 9.2-12 Potential Secondary Funding Sources

| Other Possible Sources | |
|--|---|
| Funding Resource | Description |
| Business Gross Receipts Tax | This is an excise tax on gross or net income earned by business activity. The fee amount can be based on the net income or gross receipts minus cost of goods sold. Gross receipts subject to such a tax must usually be based on local transactions. For example, Portland's tax is based on net income and is currently at a rate of 2.2% |
| County/Municipal Bonds | These are bonds issued by a county or municipality. The tax-exempt bonds are used to finance public capital improvements. The bonds may either be voted or non-voted. Bonds must fall within the city's allowable debt capacities. |
| Current Employee Tax | This can be a payroll tax or a tax on businesses in which the fee scale is based in part on the number of employees or employee income. |
| Fuels Tax | This is a tax on gasoline and diesel fuel. In Oregon, two counties and three cities have their own local fuels tax. Rates range from 1-3 cents per gallon. These funds are typically dedicated to transportation improvements. In Umatilla's case could be allocated to improvements on 6 th Street/ Hwy 730. |
| Restaurant Meals Tax | A selective sales tax on the purchase price of meals served in public establishments, this tax is added onto the patron's bill. Revenues are based on rates typically ranging from 1% to 5%. It appears the State has preempted the possibility of including alcoholic beverages in the tax. Ashland is currently the only city in the state with a meal tax. Corvallis and Seaside failed when they tried to enact this tax. |
| Special Assessments for Street Lighting, Maintenance and Cleaning | Subject to voter approval a city may collect assessments on property within its boundaries for street lighting, maintenance or cleaning which benefit the property. The associated measure needs to focus on the provision of one service. Assessments for street lighting can include an amount sufficient to pay for construction, reconstruction, modification, installation, operating and maintenance costs |
| Private Donations | Occasionally funds are made available from private sources. An endowment fund, one time or ongoing contributions can fund a position, program or street furnishing. These private contributors may include major employers or citizens. This kind of funding strategy is usually most successful for: (a) A non-profit facility like museum or interpretive center; (b) arts and cultural organizations / events, (c) for a critical economic development initiative with major private sector leadership; or (d) beautification items such as benches or fountains which can bear a plaque with the name of the contributor. |

*Note: "Blighted" indicates that because of unsafe structures, deterioration, faulty planning, harmful land use or inadequate facilities the area is detrimental to the welfare of the community.

9.2.630 *Policies and Plans*

Generally, the Downtown Revitalization Plan is consistent with the goals, findings and policies adopted in the City's Comprehensive Plan; however, the City's Plan, published in December of 1977, is outdated. To implement the Downtown Revitalization Plan, revisions to the Umatilla Comprehensive Plan and Zoning Ordinance are included under Sections 9.2.631 and 9.2.632.

9.2.631 *Umatilla Comprehensive Plan*

9.2.631(1) *Land Use:*

The Downtown Revitalization Plan is consistent with the Land Use section (Chapter 2) of the Comprehensive Plan for the reasons listed below.

The Downtown Revitalization Plan was based on the 1990 Census figures, preliminary figures released from the 2000 Census, and locally developed data from stakeholder interviews. Combined, the census figures provided sufficient information on projected population growth and economic development to use in the Downtown Revitalization Plan. As additional 2000 Census data becomes available, it may provide additional insight needed for implementation.

The location and capacity of city services is not anticipated to be an issue with the implementation of the improvements proposed in the Downtown Revitalization Plan due to the nature of the improvements. The improvements are confined to streetscape projects that should not significantly affect the physical location of utilities or their capacity. Utility location and capacity issues should be investigated at the time of new building construction or building renovation.

Information on the existing land use patterns in downtown Umatilla and surrounding areas was incorporated during the planning and analysis process. Plan recommendation carefully considered the existing land use pattern and developed recommendations based on city goals and findings.

Projected land use needs for Umatilla are documented in Chapter 14, Sections 14.2 and 14.3. This information is deemed reliable. The Downtown Revitalization Plan is consistent with the Buildable Lands Analysis. The Plan assumes growth that is well within the range outlined in the Buildable Lands Analysis. Based on this Comprehensive Plan policy, the City should consider whether the policy is met, particularly with respect to findings on natural resources and public services capacity. The City should also consider a complete revision of the Comprehensive Plan, given it was completed twenty-four years ago and the planning time horizon used was 2000.

The Downtown Revitalization Plan was developed in a manner consistent with the Comprehensive Plan goal for citizen involvement. Several means of soliciting citizen involvement were used including a questionnaire, community visioning, and numerous public workshops. More information about the citizen involvement process is included in Section 9.2.300.

Because the Downtown Revitalization Plan encourages compact growth in the downtown area, it supports the City's land use Finding 2.5.101 of minimizing development costs. The Plan is also consistent with the City's policy of encouraging new development within the area served by public utilities.

Revisions to downtown zoning designations recommended by the Downtown Revitalization Plan are intended to improve neighborhood quality and identity, as stated in Comprehensive Plan land use Finding 2.5.103. Creation of a new Downtown Residential zone in downtown Umatilla is intended to promote new types of housing compatible with Downtown Commercial activities. The zoning supports the housing policy stated in the Comprehensive Plan to provide a diversity of housing types.

The Downtown Revitalization Plan functions as an Area Plan for the downtown. The study area for the Downtown Revitalization Plan shall be used to establish the Area Plan boundary for the purposes of the Comprehensive Plan amendment and changes to the Comprehensive Plan map. The Plan boundary is described below.

The study area, or Area Plan boundary, consists of a portion of the City of Umatilla, bounded on the north by the Union Pacific Railroad and on the west and south by the Umatilla River. The area extends eastward ending at the Highway 730 overpass over the railroad tracks. The southeast portion of the area includes public school properties and playing fields and also includes two city blocks west of Sloan Avenue. The southeast portion of the area does not include land located beyond the railroad spur, which loops to the southeast to a point south of Sloan Avenue.

9.2.631(2) Transportation

The Downtown Revitalization Plan supports the transportation policies stated in the Comprehensive Plan by:

- Encouraging pedestrians circulation;
- Providing transportation alternatives to personal vehicles by creating a pedestrian and transit-friendly downtown;
- Limiting points of access along U.S. 730;
- Reviewing pedestrian circulation problems downtown; and
- Promoting adequate linkages between uses.

9.2.631(3) Urbanization

The purpose of the Downtown Revitalization Plan is directed at achieving urbanization objectives and policies stated in the Comprehensive Plan; specifically Policy 14.9.102, to promote “functional efficiency and visual attractiveness of public and private properties.”

9.2.632 Umatilla Zoning Ordinance

A careful review of the Umatilla Zoning Ordinance indicates that a few important changes to the Ordinance are required to implement the development concepts envisioned by the Downtown Revitalization Plan. No revisions to regulatory processes are required or desirable. The development approval process should remain relatively simple to encourage development.

Generally, the ordinance revisions are intended to achieve three objectives:

- Focus Downtown Commercial development where it is likely to be most successful,
- Create opportunities for a vital mixture of commercial and residential activities, and
- Define a transition of land uses from the interchange to the downtown core that will provide a wide variety of opportunities for commercial development.

The following changes implement the Plan. First, the Downtown Commercial zone has been reduced in size to concentrate development in an area that is more pedestrian in scale. Second, a new Downtown Residential zone has been designated to allow a mixture of professional office and residential uses that are compatible with the downtown core. Another new zone, Downtown Transitional (DT), has been developed that can accommodate either an extension of Downtown Commercial uses, or as a conditional use small-scale auto-oriented commercial activities. Finally, minor revisions to the Downtown Commercial zone were made to increase density and create an environment more suitable for pedestrians.

SECTION 9.3 DESTINATION MANAGEMENT PLAN

Tourism industry has significant potential in Umatilla. In 2018 City and Chamber worked together with a private consultant to identify opportunities for tourism development. After a robust public involvement process, a Destination Management Plan recommended focus on four key areas of destination development, fishing and river activities, expansion of outdoor sports complex and activities, development of a trail along the Umatilla River and clearing of invasive species along the Umatilla and Columbia Rivers to open up the viewshed to and from the river. City is working to implement the plan, which will in turn increase demand and opportunity for restaurants and retail and lodging as well as enhance the overall livability of the city.

SECTION 9.4 ECONOMIC DEVELOPMENT POLICIES

- 9.4.101 The City will provide an adequate supply of development sites, as per State requirements, to accommodate anticipated employment growth with the public and private sectors and will encourage new and continuous employment opportunities. The City will provide lands with a variety of sizes, zoning and other characteristics needed by firms likely to locate in the City of Umatilla and maintain a 20-year supply of such lands.
- 9.4.102 The City will coordinate with the Port of Umatilla, Umatilla County, Business Oregon and other local partners on local and regional economic development projects.
- 9.4.103 The City will establish districts with tax abatements, loans, subsidized infrastructure, reduced regulations or other incentives available to businesses in the district that met specified criteria.

- 9.4.104 Within the downtown area, the City of Umatilla shall encourage development of a pedestrian-oriented town center that combines both commercial and public activities and is intended to become a community gathering place.
- 9.4.105 The Downtown Commercial (DC) District is intended to provide a concentrated central business district centered on 6th Street, Highway 730. The district allows a mix of civic, retail, service, and office uses, designed to be pedestrian-friendly and oriented towards fronting streets and sidewalks.
- 9.4.106 In portions of the downtown area, the City of Umatilla shall allow for a variety of commercial opportunities accessible by pedestrians and vehicles alike that are compatible with the pedestrian scale of downtown. The Downtown Transitional (DT) District provides for a transition of uses between General Commercial uses found near the interchange and Downtown Commercial uses, which are more pedestrian in scale. The District is primarily intended to accommodate Downtown Commercial uses, which are pedestrian-friendly and oriented toward the street; however, the District is designed for greater flexibility by allowing certain General Commercial uses that require sites located closer to the downtown.
- 9.4.107 Within the downtown area, the City of Umatilla shall establish a Downtown Residential (DR) District to encourage improved land use compatibility between Downtown Commercial and residential uses. The intent of the DR District is to accommodate residential or professional office projects near the downtown core.

CITY OF UMATILLA, OREGON

AGENDA BILL

| | |
|---|------------------------------------|
| Agenda Title: Adoption of Ordinance No. 848 - An ordinance amending chapter 9 of the City of Umatilla's comprehensive land use plan to incorporate the 2019 Economic Opportunity Analysis and 2018 Destination Management Plan. | Meeting Date: 2021-08-02 |
|---|------------------------------------|

| | | | |
|---|-----------------------------------|---|----------------------|
| Department: Community Development | Director: Brandon Seitz | Contact Person: Brandon Seitz | Phone Number: |
|---|-----------------------------------|---|----------------------|

| | |
|--------------------------------|---|
| Cost of Proposal: NA | Fund(s) Name and Number(s): N/A |
| Amount Budgeted: NA | |

| | |
|--|---|
| Reviewed by Finance Department: No | Previously Presented: July 20, 2021 |
|--|---|

Attachments to Agenda Packet Item:

[Ord No. 848.docx](#)

| |
|---|
| Summary Statement: The Council approved City of Umatilla Plan Amendment PA-1-20 at their July 20, 2021, council meeting. Ordinance 848 will implement Plan Amendment PA-1-20 by amending the comprehensive plan. Staff recommends a motion to approve Ordinance No. 848. A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-1-20) is provided below or available at at City Hall during normal business hours. Umatilla City Council Report and Decision for Plan Amendment (PA-1-20) |
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| Consistent with Council Goals: Goal 2: Promote Economic Development and Job Growth. |
|---|

ORDINANCE NO. 848

AN ORDINANCE AMENDING CHAPTER 9 OF THE CITY OF UMATILLA’S COMPREHENSIVE LAND USE PLAN TO INCORPORATE THE 2019 ECONOMIC OPPORTUNITY ANALYSIS AND 2018 DESTINATION MANAGEMENT PLAN

WHEREAS, the City of Umatilla’s Comprehensive Land Use Plan was codified, reformatted and re-adopted as Ordinance No. 779 on January 8, 2013; and

WHEREAS, The City of Umatilla made application for a comprehensive plan amendment under application PA-1-20 to incorporate the results of the 2019 Economic Opportunities Analysis prepared by Johnson Economics; and

WHEREAS, the Umatilla Planning Commission held a public hearing on August 25, 2020 and September 22, 2020, to consider Plan Amendment PA-1-20 an amendment of Chapter 9 of the City’s Comprehensive Plan, and recommended approval to the Umatilla City Council; and

WHEREAS, the Umatilla City Council conducted a public hearing on July 20, 2021, to consider the Planning Commission’s recommendation for Plan Amendment PA-1-20 and adopted the Planning Commission’s findings and conclusions as its own in approving the application, as contained in the *Umatilla City Council Report and Decision for Plan Amendment PA-1-20*.

NOW THEREFORE, THE CITY OF UMATILLA DOES ORDAIN AS FOLLOWS:

Section 1. Findings. The Umatilla City Council does hereby adopt the findings and conclusions recommended by the City Planning Commission as its own in support of this amendment to the Umatilla Comprehensive Plan, as contained in the *Umatilla City Council Report and Decision for Plan Amendment, PA-1-19*.

Section 2. Comprehensive Plan amendment. The Umatilla City Council does hereby amend Ordinance No 779 by adopting an update to Chapter 9 (Economic Development) of the City of Umatilla’s Comprehensive Plan, as attached hereto as Exhibit A.

Section 3. Severability. If any provision of this Ordinance is held to be invalid by a court of competent jurisdiction, the invalidity shall not affect other provisions of this Ordinance that can be given effect without the invalid provision, and to this end the provisions of this Ordinance are severable.

Section 4. Effective date. The effective date of this Ordinance shall be 30 days after its passage by the city council and approval by the mayor, or by the council president in the mayor’s absence.

ADOPTED by the City Council this ___ day of August, 2021.

Council members voting yes: _____

Council members voting no: _____

Absent Council members: _____

Abstaining Council members: _____

And **SIGNED** by the Mayor this ____ day of August, 2021.

Mary Dedrick, Mayor

ATTEST:

Nanci Sandoval, City Recorder

CITY OF UMATILLA, OREGON

AGENDA BILL

| | |
|--|------------------------------------|
| Agenda Title: First Reading of Ordinance No. 849 - An ordinance amending the City of Umatilla Comprehensive Plan and Zoning map by changing the plan designation of 294 acres from Single-Family Residential (R-1) to Light Industrial (M-1) | Meeting Date: 2021-08-02 |
|--|------------------------------------|

| | | | |
|---|-----------------------------------|---|----------------------|
| Department: Community Development | Director: Brandon Seitz | Contact Person: Brandon Seitz | Phone Number: |
|---|-----------------------------------|---|----------------------|

| | |
|--------------------------------|---|
| Cost of Proposal: NA | Fund(s) Name and Number(s): N/A |
| Amount Budgeted: NA | |

| | |
|--|------------------------------------|
| Reviewed by Finance Department: No | Previously Presented: NA |
|--|------------------------------------|

Attachments to Agenda Packet Item:

[Ord No 849.pdf](#)

| |
|---|
| Summary Statement: The Council approved City of Umatilla Plan Amendment PA-1320 at their July 20, 2021, council meeting. Ordinance 849 will implement Plan Amendment PA-3-20 by amending the Comprehensive Plan and Zoning map. Staff recommend calling for a first reading for Ordinance No. 849 by title only. A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-3-20) is provided below or available at at City Hall during normal business hours. Umatilla City Council Report and Decision for Plan Amendment (PA-3-20) |
|---|

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|---|
| Consistent with Council Goals: Goal 2: Promote Economic Development and Job Growth. |
|---|

ORDINANCE NO. 849

AN ORDINANCE AMENDING THE CITY OF UMATILLA COMPREHENSIVE PLAN AND ZONING MAP BY CHANGING THE PLAN DESIGNATION OF 294 ACRES FROM SINGLE-FAMILY RESIDENTIAL (R-1) TO LIGHT INDUSTRIAL (M-1)

WHEREAS, Cleaver Land, LLC, made application for a plan amendment/zone change to change the plan and zoning designation of 294 acres of property in the Single-Family Residential (R-1) to Light Industrial (M-1); and

WHEREAS, the City of Umatilla Planning Commission duly considered the goals and policies of the Comprehensive Plan and requirements of the Zoning Ordinance as those applied to the application during public hearings on August 25, 2020 and September 22, 2020 to consider Plan Amendment (PA-3-20) and subsequently recommended approval of the request to the City Council; and

WHEREAS, the Umatilla City Council conducted a public hearing on July 20, 2020 to consider the Planning Commission’s recommendation for application PA-3-20 and adopted the Planning Commission’s findings and conclusion as its own in approving the application, as contained in the Umatilla City Council Report and Decision for Plan Amendment PA-3-20

NOW THEREFORE, THE CITY OF UMATILLA DOES ORDAIN AS FOLLOWS:

Section 1. Findings. The Umatilla City Council does hereby find that the applicant meets the required criterion as addressed in *Umatilla City Council Report and Decision for Plan Amendment PA-3-20*.

Section 2. Comprehensive Plan and Zoning Map Amendment. The Comprehensive Plan and Zoning Map designation for the 294-acre property, as shown in Exhibit A attached to his Ordinance, is amended to reflect the City of Umatilla’s Light Industrial (M-1) zone.

Section 3. Setting the final boundaries. The final boundaries of the Property hereby rezoned are legally described as follows:

(Commonly known as Tax Lots 1400 on Assessors Map 5N28C; Tax Accounts # 124632)

TOWNSHIP 5 NORTH, RANGE 28, E.W.M.

Section 29: East Half.
East half of the West Half.
Excepting therefrom that portion laying Easterly of US I-82.
Excepting therefrom that portion laying outside Umatilla City Limits.

Section 4. Severability. If any provision of this Ordinance is held to be invalid by a court of competent jurisdiction, the invalidity shall not affect other provisions of this Ordinance that can be given effect without the invalid provision, and to this end the provisions of this Ordinance are severable.

Section 5. Effective date. The effective date of this ordinance shall be 30 days after its passage by the city council and approval by the mayor, or by the council president in the mayor's absence.

PASSED AND ADOPTED by the City Council this 2nd day of August, 2021.

Council members voting yes: _____

Council members voting no: _____

Absent Council members: _____

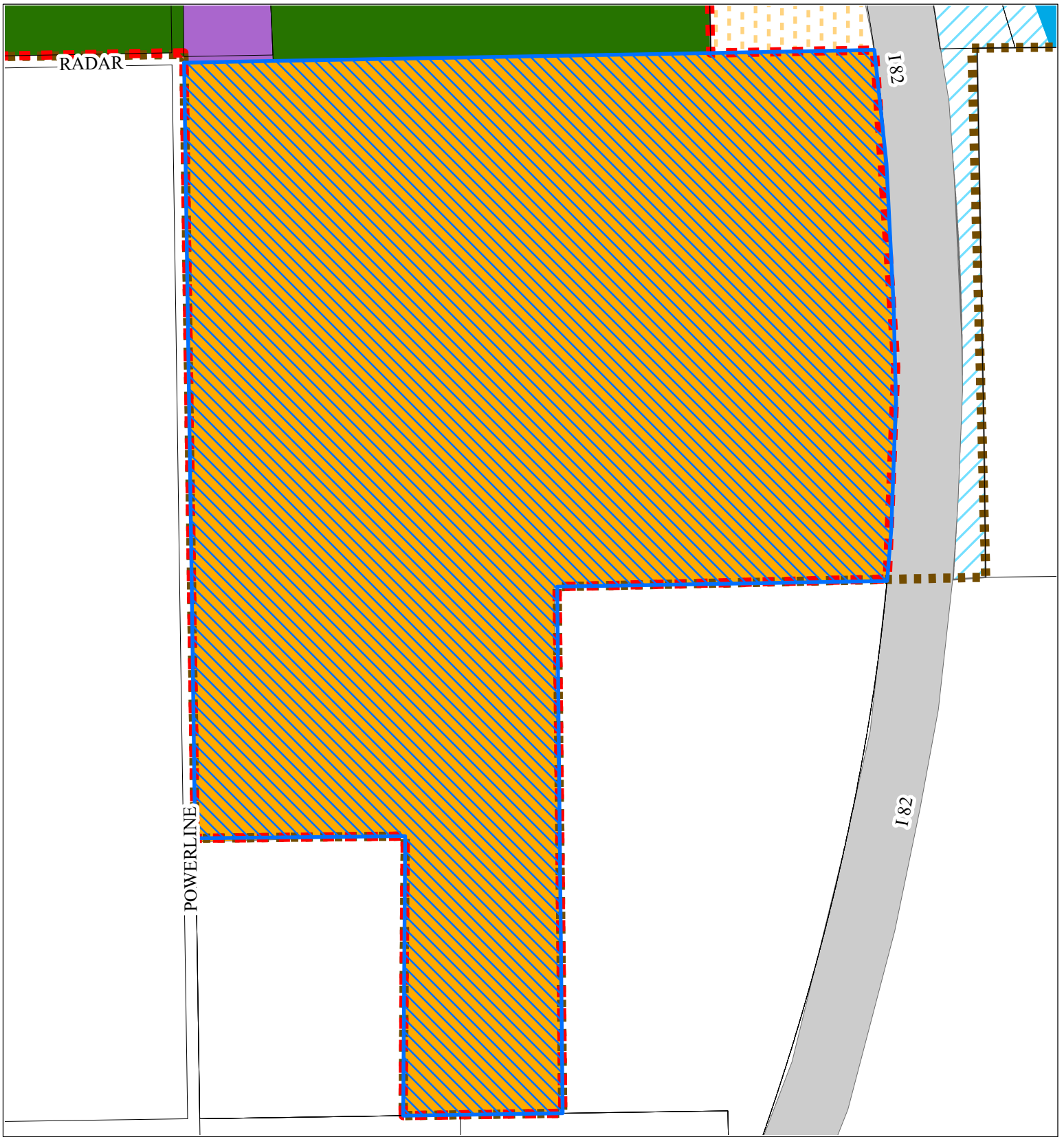
Abstaining Council members: _____

And **SIGNED** by the Mayor/Council President this 2nd day of August, 2021.

Mary Dedrick, Mayor











ATTEST:

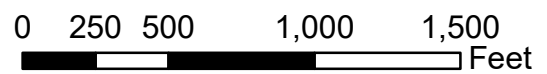
Nanci Sandoval, City Recorder



ORD NO. 849 EXHIBIT A

Legend

- | | | | |
|--|---------------------------|---|-----------------------------------|
|  | Proposed Light Industrial |  | R-2, Medium Density Residential |
|  | Tax Lots (6/2/21) |  | NC, Neighborhood Commercial |
|  | City Limits |  | R-1, Single-Family Residential |
|  | Urban Growth Boundary |  | R, Residential Plan |
| | |  | C, Commercial Plan |
| | |  | R-O/S, Recreation-Open/Space Plan |



MAP DISCLAIMER: No warranty is made as to the accuracy, reliability or completeness of this data. Map should be used for reference purposes only. Not survey grade or for legal use. Created by Brandon Seitz, on 7/29/2021

CITY OF UMATILLA, OREGON

AGENDA BILL

| | |
|---|------------------------------------|
| Agenda Title: Adoption of Ordinance No. 849 - An ordinance amending the City of Umatilla Comprehensive Plan and Zoning map by changing the plan designation of 294 acres from Single-Family Residential (R-1) to Light Industrial (M-1) | Meeting Date: 2021-08-02 |
|---|------------------------------------|

| | | | |
|---|-----------------------------------|---|----------------------|
| Department: Community Development | Director: Brandon Seitz | Contact Person: Brandon Seitz | Phone Number: |
|---|-----------------------------------|---|----------------------|

| | |
|--------------------------------|---|
| Cost of Proposal: NA | Fund(s) Name and Number(s): N/A |
| Amount Budgeted: NA | |

| | |
|--|------------------------------------|
| Reviewed by Finance Department: No | Previously Presented: NA |
|--|------------------------------------|

Attachments to Agenda Packet Item:

[Ord No 849.pdf](#)

| |
|--|
| Summary Statement: The Council approved City of Umatilla Plan Amendment PA-3-20 at their July 20, 2021, council meeting. Ordinance 849 will implement Plan Amendment PA-3-20 by amending the Comprehensive Plan and Zoning map. A link to the Umatilla City Council Report and Decision for Plan Amendment (PA-3-20) is provided below or available at at City Hall during normal business hours. Umatilla City Council Report and Decision for Plan Amendment (PA-3-20) |
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| |
|---|
| Consistent with Council Goals: Goal 2: Promote Economic Development and Job Growth. |
|---|

ORDINANCE NO. 849

AN ORDINANCE AMENDING THE CITY OF UMATILLA COMPREHENSIVE PLAN AND ZONING MAP BY CHANGING THE PLAN DESIGNATION OF 294 ACRES FROM SINGLE-FAMILY RESIDENTIAL (R-1) TO LIGHT INDUSTRIAL (M-1)

WHEREAS, Cleaver Land, LLC, made application for a plan amendment/zone change to change the plan and zoning designation of 294 acres of property in the Single-Family Residential (R-1) to Light Industrial (M-1); and

WHEREAS, the City of Umatilla Planning Commission duly considered the goals and policies of the Comprehensive Plan and requirements of the Zoning Ordinance as those applied to the application during public hearings on August 25, 2020 and September 22, 2020 to consider Plan Amendment (PA-3-20) and subsequently recommended approval of the request to the City Council; and

WHEREAS, the Umatilla City Council conducted a public hearing on July 20, 2020 to consider the Planning Commission’s recommendation for application PA-3-20 and adopted the Planning Commission’s findings and conclusion as its own in approving the application, as contained in the Umatilla City Council Report and Decision for Plan Amendment PA-3-20

NOW THEREFORE, THE CITY OF UMATILLA DOES ORDAIN AS FOLLOWS:

Section 1. Findings. The Umatilla City Council does hereby find that the applicant meets the required criterion as addressed in *Umatilla City Council Report and Decision for Plan Amendment PA-3-20*.

Section 2. Comprehensive Plan and Zoning Map Amendment. The Comprehensive Plan and Zoning Map designation for the 294-acre property, as shown in Exhibit A attached to his Ordinance, is amended to reflect the City of Umatilla’s Light Industrial (M-1) zone.

Section 3. Setting the final boundaries. The final boundaries of the Property hereby rezoned are legally described as follows:

(Commonly known as Tax Lots 1400 on Assessors Map 5N28C; Tax Accounts # 124632)

TOWNSHIP 5 NORTH, RANGE 28, E.W.M.

Section 29: East Half.
East half of the West Half.
Excepting therefrom that portion laying Easterly of US I-82.
Excepting therefrom that portion laying outside Umatilla City Limits.

Section 4. Severability. If any provision of this Ordinance is held to be invalid by a court of competent jurisdiction, the invalidity shall not affect other provisions of this Ordinance that can be given effect without the invalid provision, and to this end the provisions of this Ordinance are severable.

Section 5. Effective date. The effective date of this ordinance shall be 30 days after its passage by the city council and approval by the mayor, or by the council president in the mayor's absence.

PASSED AND ADOPTED by the City Council this 2nd day of August, 2021.

Council members voting yes: _____

Council members voting no: _____

Absent Council members: _____

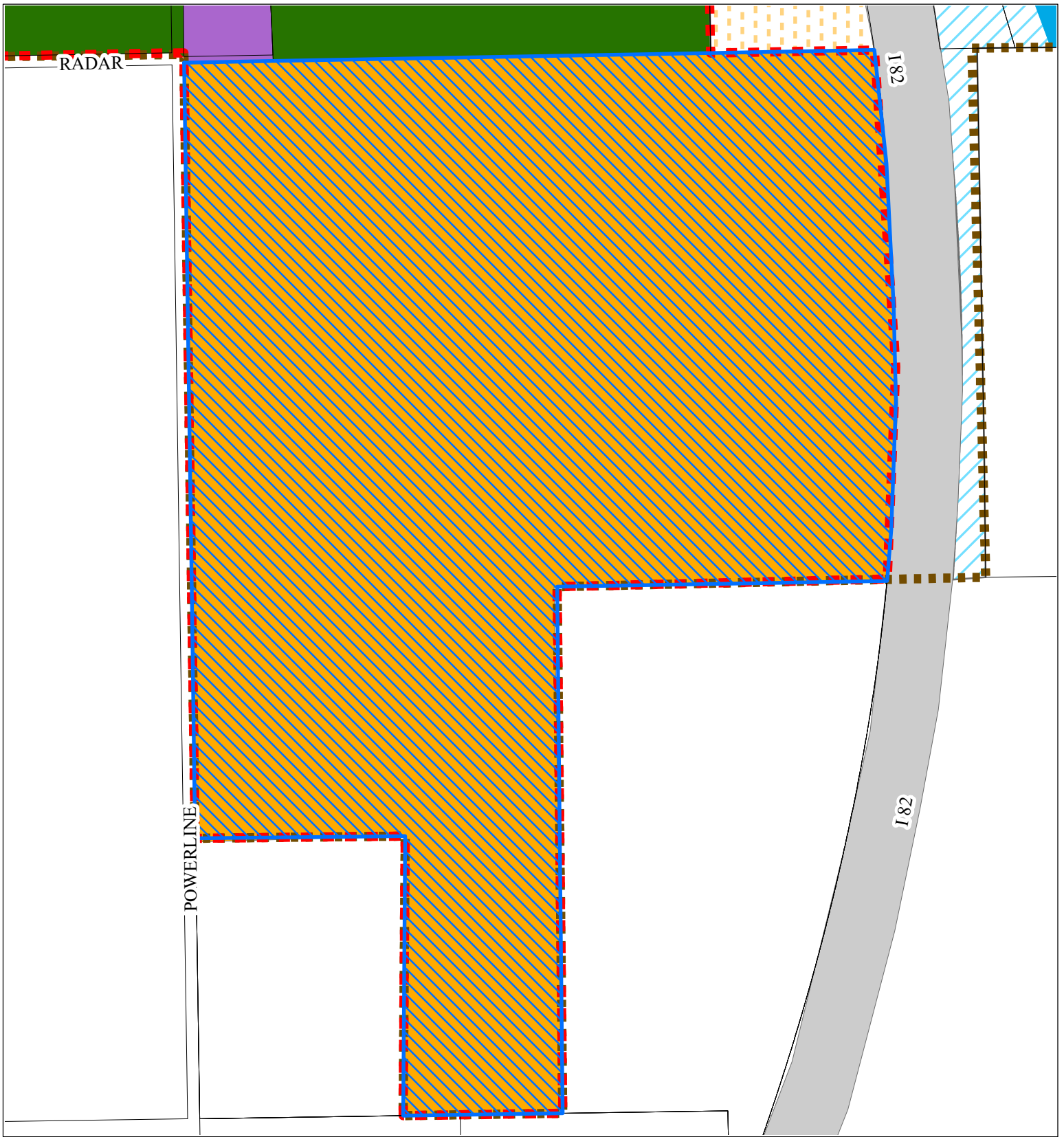
Abstaining Council members: _____

And **SIGNED** by the Mayor/Council President this 2nd day of August, 2021.

Mary Dedrick, Mayor











ATTEST:

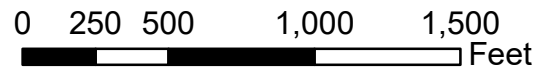
Nanci Sandoval, City Recorder



ORD NO. 849 EXHIBIT A

Legend

-  Proposed Light Industrial
-  R-2, Medium Density Residential
-  NC, Neighborhood Commercial
-  R-1, Single-Family Residential
-  R, Residential Plan
-  C, Commercial Plan
-  R-O/S, Recreation-Open/Space Plan
-  Tax Lots (6/2/21)
-  City Limits
-  Urban Growth Boundary



MAP DISCLAIMER: No warranty is made as to the accuracy, reliability or completeness of this data. Map should be used for reference purposes only. Not survey grade or for legal use. Created by Brandon Seitz, on 7/29/2021

CITY OF UMATILLA, OREGON

AGENDA BILL

Agenda Title:

Resolution No. 06-2022 - A resolution authorizing the city manager to sign a developer agreement for street improvement funds between Fastrack Inc, Columbia Basin Development LLC and the City of Umatilla

Meeting Date:

2021-08-02

Department:

Community Development

Director:

Brandon Seitz

Contact Person:

Brandon Seitz

Phone Number:

Cost of Proposal:

NA

Amount Budgeted:

NA

Fund(s) Name and Number(s):

N/A

Reviewed by Finance Department:

No

Previously Presented:

NA

Attachments to Agenda Packet Item:

[Resolution_6-2022.docx](#)

[Fastrack & CBD Development Agreement.pdf](#)

Summary Statement:

Staff recommends approval of Resolution 06-2021.

Consistent with Council Goals:

Goal 2: Promote Economic Development and Job Growth.

RESOLUTION NO. 06 – 2022

A RESOLUTION AUTHORIZING THE CITY MANAGER TO SIGN A DEVELOPER AGREEMENT FOR STREET IMPROVEMENT FUNDS BETWEEN FASTRACK INC, COLUMBIA BASIN DEVELOPMENT LLC AND THE CITY OF UMATILLA

WHEREAS, Fastrack Inc, a Washington Corporation, and Columbia Basin Development LLC, a Washington Limited Liability Corporation submitted land use applications for development of subdivisions on property located within City Limits of the City of Umatilla (City); and

WHEREAS, the City approved said land use applications (Ballard Subdivision, Vandalay Meadows and Cheryl's Place) for development of subdivisions totaling 389 lots; and

WHEREAS, Fastrack Inc and Columbia Basin Development LLC submitted a joint Transportation Impact Analysis that identified needed improvements to the Powerline Road/US Hwy 730 intersection; and

WHEREAS, the City imposed conditions of approval requiring the applicant pay their proportionate share of said improvement; and

WHEREAS, the City desires to meet the housing need as identified in the City's Comprehensive Plan and Housing and Residential Land Needs Assessment of the City and has determined that it is in the best interest of the City to financially partner with Fastrack Inc and Columbia Basin Development LLC.

NOW THEREFORE, BE IT RESOLVED BY THE UMATILLA CITY COUNCIL:

1. The City Manager is authorized to sign a Developer Agreement by and between Fastrack Inc, a Washington Corporation, Columbia Basin Development LLC, a Washington Limited Liability Corporation, and the City of Umatilla, an Oregon municipal corporation,

PASSED by the City Council and **SIGNED** by the Mayor this 2nd day of August, 2021.

Mary Detrick, Mayor

ATTEST:

Nanci Sandoval, City Recorder

**DEVELOPER AGREEMENT
(Street Improvement Funds)**

THIS AGREEMENT made this _____ day of August, 2021, by and between Fastrack Inc. (“Fastrack”), a Washington Corporation, Columbia Basin Development LLC (“Columbia Basin”), a Washington Limited Liability Corporation, (collectively as “Developers”) and the City of Umatilla (“City”), an Oregon municipal corporation (collectively as “the parties”).

RECITALS

- A. Developers anticipate the development of three tracts of land located in the city limits of the City of Umatilla as a multi-phased project for residential purposes. These tracts of land are described in Exhibit 1 and shown on the map attached as Exhibit 2. These lands are separated into three developments “Ballard Subdivision”, “Vandalay Meadows” and “Cheryl’s Place”.
- B. The City desires to meet the housing needs of the City of Umatilla and has determined that it is in the best interest of the City to financially partner with Developer to help the City meet these goals and to do so through the utilization of the reduction of some fees as established by this agreement.
- C. The parties acknowledge that Developer’s residential development will increase the usage of Powerline Road. This agreement shall provide a method for Developer to make financial contributions for future improvements of Powerline Road at the identified intersection of Powerline Road at US Hwy 730.

AGREEMENTS

NOW THEREFORE, in consideration of the mutual promises and stipulations set forth herein, it is agreed between the parties as follows:

General Acknowledgements

- 1. In order to serve the land development proposed by Developers and approved by City, the parties mutually confirm that improvements to the Powerline Road/US Hwy 730 intersection are necessary. Developers agrees to provide the Developer’s proportionate share of the cost of these necessary improvements as set for in Paragraphs 16.
- 2. Developers submitted a Transportation Impact Analysis (TIA) prepared by PBS Engineering that was approved by City. The TIS calculated the development would generate 57% of the total increase in trips from the 2020 existing conditions to 2030 with project conditions.

3. The estimated cost for the required improvements for the Powerline Road/US 730 intersection is \$6,000,000.
4. Developers may transfer or assign this agreement without the consent of the City provided, Developers provides written notification to City not less than ten (10) days prior to such transfer or assignment.
5. The parties acknowledge that Developer's development referred to herein may occur over several years. The parties agree that the price per lot established herein shall remain fixed regardless of any increased costs resulting from the passage of time.
6. In the event any legal proceeding and/or appeal is necessary to enforce the provisions of this agreement, the prevailing party shall be entitled to reasonable attorney's fees and costs.
7. This agreement, and the rights and obligations created under it run with the land (defined above as Ballard Subdivision, Vandalay Meadows and Cheryl's Place) and shall benefit and burden the parties, their heirs, successors, transferees and assigned parties.
8. It is the express intent of Developers and City to cooperate and diligently work to implement all aspects of the future street improvements which are necessary or desirable for development of the project site.
9. The parties agree that any funds paid by Developer pursuant to this agreement, which have not been expended by the City for improvement to Powerline Road within twenty (20) years from the date of this agreement, shall be released to Developers free of any claim by the City. Developers waives any interest accrued.

Powerline Road/US Hwy 730 Proportionate Share

10. The parties acknowledge Developer's TIA report established that Developers should contribute to a future improvement at the Powerline Road and US Hwy 730 intersection. The amount of the contribution should be based on the increase in traffic and the cost of improvements that will meet ODOT operational standards.
11. The parties acknowledge Developer's TIA considered Phase 1 of Cheryl's Place and Vandalay Meadows as in process, as shown on attached Exhibit 3. Therefore, those lots shall not be subject to the per lot traffic fee set forth in Paragraph 16.
12. City desires to share the financial burden of transportation improvements to Powerline Road/US Hwy 730 with Developer in order to offset the increased costs to potential homeowners to help meet City's goals of providing needed additional housing throughout the City. City has determined it to be in the best interest of the City to reduce the overall

costs associated with this improvement by 55%. This amount reflects the estimated amount City believes it will be able to receive through grants or a combination of grants and City funds.

13. The parties acknowledge that other residential developments have been permitted by the City that further reduce the Developers proportionate share of improvements at Powerline Road and US Hwy 730.
14. City calculated proportionate share of traffic at Powerline Road and US Hwy 730 for various developments and found Developers proportionate share as follows:
 - a. Ballard Subdivision - 11.6%
 - b. Cheryl's Place – 3.1%
 - c. Vandalay Meadows – 1.5%
15. Developers have requested that the Developer's proportionate share be divided evenly over the entire project and not by individual developments.
16. Developer, therefore, shall pay to the City the sum of **\$756.75 per lot** payable, upon issuance of a building permit on each lot, into a Capital Improvement Account to be set aside for Powerline Road/US Hwy 730 intersection improvements.

Future System Development Charges

17. In consideration for the proportionate share payments Developer is obligating itself and any successors and assigns to make under this Agreement, the City agrees and acknowledges that the lots within the development will never be subject to any future System Development Charges for transportation improvements.

WHEREAS, the undersigned have been duly authorized to enter into this Agreement on behalf of their respective corporations, on the day and year first above written.

FASTRACK INC.
A Washington Corporation

COLUMBIA BASIN DEVELOPMENT LLC
A Washington Limited Liability Corporation

Jared Faris, Governor

Aaron Riley, Governor

CITY OF UMATILLA, OREGON

Dave Stockdale, City Manager

ATTEST:

Nanci Sandoval, City Recorder

DEVELOPER AGREEMENT
Exhibit 1

The final boundaries of the Properties are legally described as follows:

Ballard Subdivision

Tax Lots 2300 on Assessors Map 5N2820; Tax Account # 154687

Lot 41, HAYDEN RIVER ESTATES NO.3, a Replat of Lot 34 in Hayden River Estates No. 2A, laying within a portion of Section 20, Township 5 North, Range 28, East of the Willamette Meridian, City of Umatilla, Umatilla County, Oregon.

Tax Lot 1700 on Assessors Map 5N2820; Tax Account #157205

A tract of land located in Parcel 3, Partition Plat No. 2 000 ~ 3 7 and in the West Half of the Southeast Quarter of Section 20, being described as follows:

A tract of land located in Section 20, Township 5 North, Range 28, East of the Willamette Meridian, Umatilla County, Oregon and being more particularly described as follows:

Beginning at the Center Quarter Corner of said Section 20; thence along the North-South centerline of said Section 20, South 00°06'26" East, a distance of 657.67 feet; thence North 89°48'44" East a distance of 1324.40 feet, to a point on the East line of the South Half of the Northwest Quarter of the Southeast Quarter of said Section 20; thence along said line, South 00°13'13" East, a distance of 212.67 feet; thence South 89°48'36" West, a distance of 2619.53 feet, to a point on the Easterly right-of-way of Powerline Road; thence along said right-of-way, North 00°00'38" East, a distance of 750.41 feet, to a point on the Southerly boundary of Lot 41, Hayden River Estates Phase 3, as per the Plat thereof in Book 14, Page 13, Plat Records, Umatilla County, Oregon; thence along said Southerly line, the following 10 calls: (1) South 89°59'08" East, a distance of 111.09 feet; (2) North 87°46'49" East, a distance of 60.05 feet; (3) South 89°59'08" East, a distance of 100.00 feet; (4) North 00°00'52" East, a distance of 69.11 feet; (5) North 43°07'46" East, a distance of 61.78 feet; (6) North 34°37'08" East, a distance of 100.38 feet; (7) South 55°24'49" East, a distance of 686.04 feet; (8) North 34°35'11" East, a distance of 100.00 feet; (9) North 66°56'02" East, a distance of 71.02 feet; (10) North 34°35'11" East, a distance of 414.69 feet to a point on the North-South centerline of said Section 20; thence along said line, South 00°06'26" East, a distance of 136.93 feet to the point of beginning.

Tax Lot 1300 on Assessors Map 5N2820; Tax Account #114871

The West Half of the Southwest Quarter of the Northeast Quarter of Section 20, Township 5 North, Range 28, East, Willamette Meridian, County of Umatilla, State of Oregon;

EXCEPTING THEREFROM the North 447.42 feet of the West 208.71 feet thereof;

ALSO EXCEPTING THEREFROM that portion described in deed recorded October 30, 1997 in Microfilm R-318, Page 597, Office of County Records, Umatilla County, Oregon.

Tax Lot 1400 on Assessors Map 5N2820; Tax Account #114876

The East Half of the Southwest Quarter of the Northeast Quarter of Section 20, Township 5 North, Range 28, East, Willamette Meridian, County of Umatilla, State of Oregon.

Tax Lot 1600 on Assessors Map 5N2820; Tax Account #114878

The North Half of the Northwest Quarter of the Southeast Quarter of Section 20, Township 5 North, Range 28, East, Willamette Meridian, County of Umatilla, State of Oregon.

Cheryl's Place

Tax Lot 100 on Assessors Map 5N2820CC; Tax Account #154123

Lot 75 of The Bluff's Subdivision, as shown on the plat of "The Bluffs" Subdivision, Phase 1, recorded on August 15, 2003 as Instrument Number 2003-4450088 in Book 14 at Page 63 in the plat records of Umatilla County, Oregon.

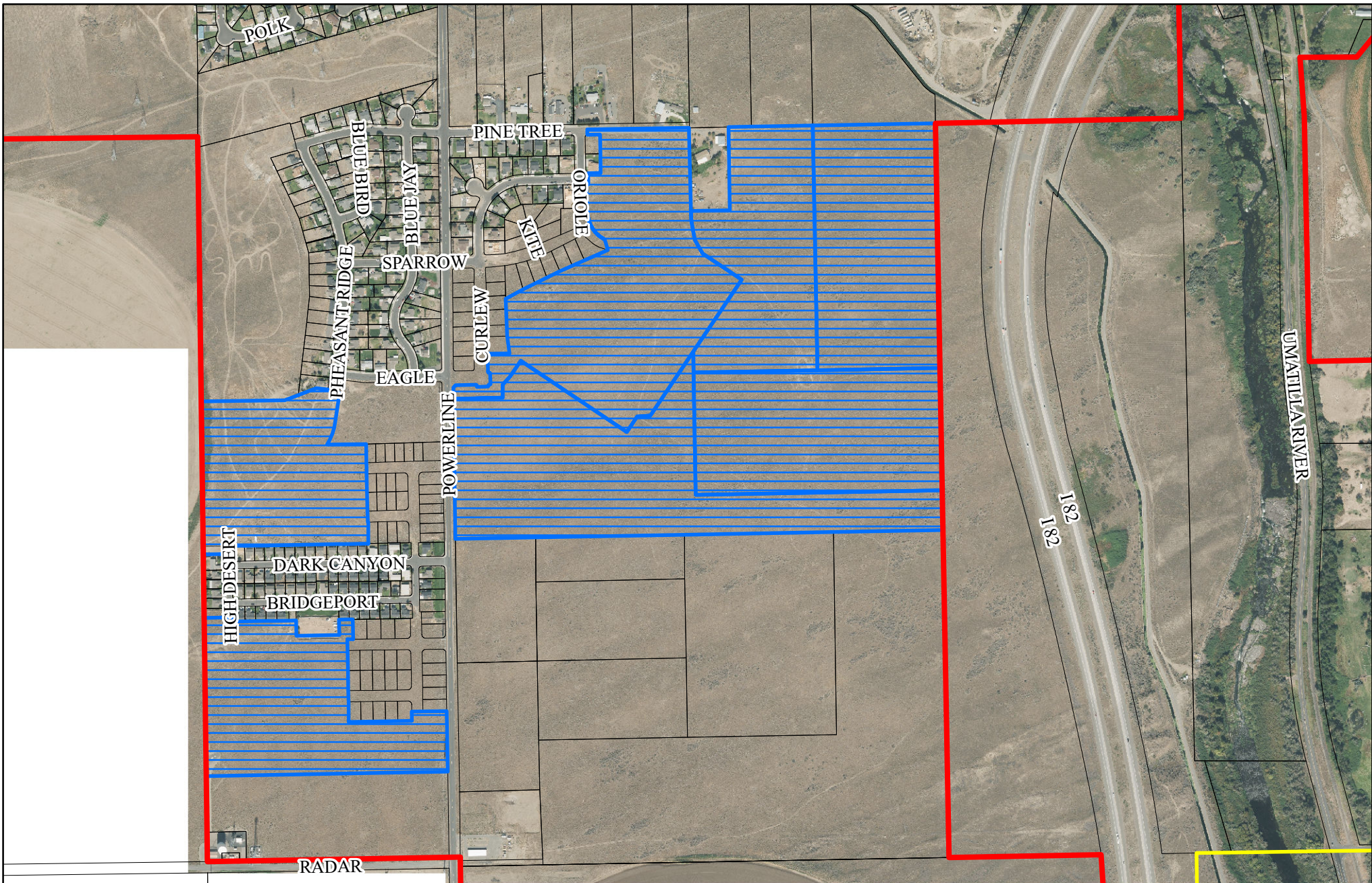
Vandalay Meadows

Tax Lot 200 on Assessors Map 5N2820CB; Tax Account #159026





Lot 76 of The Bluff's Subdivision, as shown on the plat of "The Bluffs" Subdivision, Phase 1, recorded on August 15, 2003 as Instrument Number 2003-4450088 in Book 14 at Page 63 in the plat records of Umatilla County, Oregon.

Tax Lot 100 on Assessors Map 5N2820CB; Tax Account #159025

Lot 77 of The Bluff's Subdivision, as shown on the plat of "The Bluffs" Subdivision, Phase 1, recorded on August 15, 2003 as Instrument Number 2003-4450088 in Book 14 at Page 63 in the plat records of Umatilla County, Oregon.



Legend

-  Subject Properties
-  City Limits
-  Tax Lots (6/2/21)
-  Urban Growth Boundary

**Fastrack & Columbia Basin Development Developer Agreement
Exhibit 2**

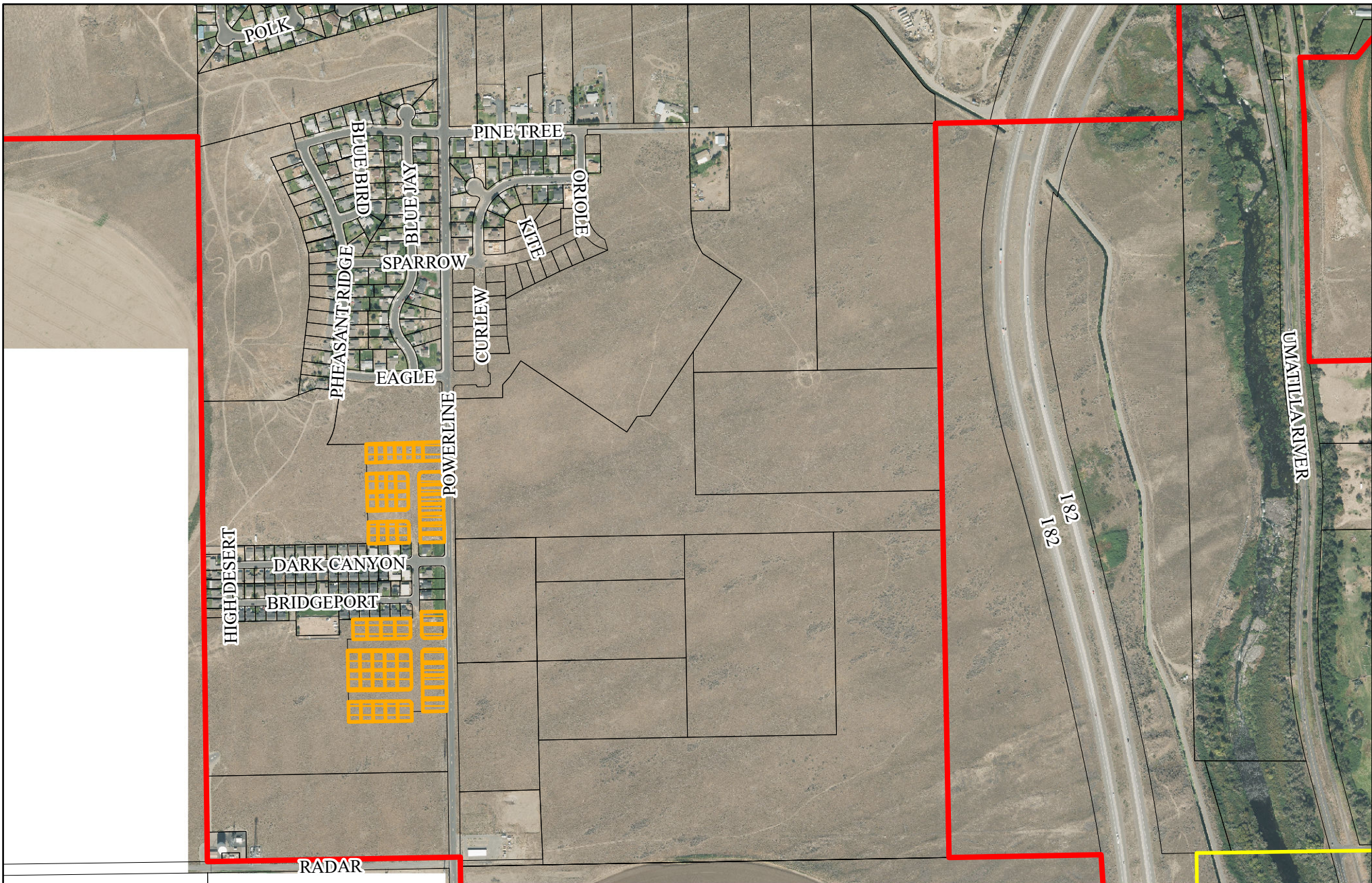
Feet

0 250 500 750 1,000







MAP DISCLAIMER: No warranty is made as to the accuracy, reliability or completeness of this data. Map should be used for reference purposes only.

Not survey grade or for legal use. Created by Brandon Seitz, on 7/30/2021 **176**



Legend

-  Subject Properties
-  City Limits
-  Tax Lots (6/2/21)
-  Urban Growth Boundary

**Fastrack & Columbia Basin Development Developer Agreement
Exhibit 3**

Feet
0 250 500 750 1,000



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Not survey grade or for legal use. Created by Brandon Seitz, on 7/30/2021 **177**

CITY OF UMATILLA, OREGON

AGENDA BILL

| | |
|--|------------------------------------|
| Agenda Title: Potential Real Estate Transaction - ORS 192.660(2)(e) Authorizes council to deliberate with persons designated by council to negotiate real property transactions, including long-term leases. Does not authorize discussion of general leasing policies. | Meeting Date: 2021-08-02 |
|--|------------------------------------|

| | | | |
|---|-------------------------------------|---|----------------------|
| Department: City Administration | Director: David Stockdale | Contact Person: David Stockdale | Phone Number: |
|---|-------------------------------------|---|----------------------|

| | |
|---------------------------------|---|
| Cost of Proposal: n/a | Fund(s) Name and Number(s): General Fund - 01 |
| Amount Budgeted: n/a | |

| | |
|---|-------------------------------------|
| Reviewed by Finance Department: Yes | Previously Presented: n/a |
|---|-------------------------------------|

Attachments to Agenda Packet Item:

| |
|--|
| Summary Statement: Discussion Only |
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|---|
| Consistent with Council Goals: Goal 2: Promote Economic Development and Job Growth. |
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CITY OF UMATILLA, OREGON

AGENDA BILL

Agenda Title:

Potential Real Estate Transaction - ORS 192.660(2)(e)
Authorizes council to deliberate with persons designated by council to negotiate real property transactions, including long-term leases. Does not authorize discussion of general leasing policies.

Meeting Date:

2021-08-02

Department:

City Administration

Director:

David Stockdale

Contact Person:

David Stockdale

Phone Number:

Cost of Proposal:

n/a

Amount Budgeted:

n/a

Fund(s) Name and Number(s):

General Fund - 01

Reviewed by Finance Department:

Yes

Previously Presented:

n/a

Attachments to Agenda Packet Item:

Summary Statement:

Discussion Only

Consistent with Council Goals:

Goal 2: Promote Economic Development and Job Growth.

CITY OF UMATILLA, OREGON

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General Fund - 01

Reviewed by Finance Department:

Yes

Previously Presented:

n/a

Attachments to Agenda Packet Item:

Summary Statement:

Discussion Only

Consistent with Council Goals:

Goal 2: Promote Economic Development and Job Growth.

CITY OF UMATILLA, OREGON

AGENDA BILL

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| Amount Budgeted: n/a | |

| | |
|---|-------------------------------------|
| Reviewed by Finance Department: Yes | Previously Presented: n/a |
|---|-------------------------------------|

Attachments to Agenda Packet Item:

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|--|
| Summary Statement: Discussion Only |
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|---|
| Consistent with Council Goals: Goal 1: Promote a Vibrant and Growing Community by Investing in and Support of Quality of Life Improvements. |
|---|